

Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site

Prepared for the Township of Plainsboro, New Jersey

Phillips Preiss Grygiel LLC | Planning & Real Estate Consultants | Adopted March 13, 2013

AMENDED REDEVELOPMENT PLAN FOR
THE PRINCETON HEALTHCARE SYSTEM AT PLAINSBORO SITE*

Prepared for the Township of Plainsboro

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Adopted March 13, 2013

* The original Redevelopment Plan was entitled "Redevelopment Plan for the FMC Corporation Site", prepared by Phillips Preiss Shapiro Associates, Inc., January 23, 2008

ORDINANCE OF THE TOWNSHIP OF PLAINSBORO, IN THE COUNTY OF MIDDLESEX APPROVING AND ADOPTING AMENDMENTS TO THE FMC SITE RE-DEVELOPMENT PLAN

WHEREAS, the Local Redevelopment and Housing Law, *N.J.S.A. 40A:12A-1 et seq.* (the “Redevelopment Law”) authorizes municipalities to determine whether certain parcels of land located therein constitute areas in need of redevelopment; and

WHEREAS, by Resolution adopted on September 10, 2007, the Plainsboro Township Committee designated the area consisting of Block 1701, Lots 3 and 4, Block 1703, Lots 1, 2 and 3, and Block 1704, Lot 1 (“FMC Site”) as an area in need of redevelopment; and

WHEREAS, by Ordinance adopted on January 23, 2008, the Plainsboro Township Committee approved and adopted a redevelopment plan for the FMC Site, entitled “Redevelopment Plan for FMC Corporation Site” (the “FMC Site Redevelopment Plan”); and

WHEREAS, Plainsboro Township (“the Township”) entered into a redevelopment agreement (the “Redevelopment Agreement”), dated July 7, 2008, with Princeton HealthCare System, a New Jersey Nonprofit Corporation (the “Redeveloper”), which sets forth the obligations of the Township and the Redeveloper in connection with the redevelopment of the FMC Site; and

WHEREAS, in accordance with the Redevelopment Agreement, the Redeveloper, through itself and Partner Developers (as defined in the Redevelopment Agreement), has effectuated the redevelopment of a portion of the FMC Site, specifically, the Hospital Portion, the SNF Portion and the Medical Office Portions (as such terms are defined in the Redevelopment Agreement); and

WHEREAS, by Resolution adopted on October 10, 2012, the Township Committee authorized and directed the Plainsboro Township Planning Board (the “Planning Board”) to conduct a preliminary investigation to determine whether an adjacent parcel designated as Block 1701, Lots 1 & 2 on the Township tax maps (the “Crescent Parcel”) meets the criteria set forth in the Redevelopment Law for redevelopment area designation and to make a recommendation as to whether such property should be designated as an area in need of redevelopment; and

WHEREAS, the Planning Board caused Phillips Preiss Grygiel LLC (the “Planning Consultant”) to conduct a preliminary investigation to determine whether the Crescent Parcel should be designated an area in need of redevelopment and incorporated into the FMC Site redevelopment area; and

WHEREAS, on November 9, 2012, the Planning Board received a report and map depicting the Crescent Parcel prepared by the Planning Consultant, entitled “Area in Need of Redevelopment Investigation, Block 1701, Lots 1 & 2” and dated October 2012 (the “Report”), concerning the determination of the Crescent Parcel as an area in need of redevelopment; and

WHEREAS, on November 19, 2012, the Planning Board reviewed the Report, heard testimony from representatives of the Planning Consultant, conducted a public hearing during which members of the general public were given an opportunity to present their own evidence and/or to

cross-examine the Planning Consultant concerning the potential designation of the Crescent Parcel as an area in need of redevelopment; and

WHEREAS, the Planning Consultant concluded in the Report and testified to the Planning Board on November 19, 2012 that the Crescent Parcel satisfies the criterion for redevelopment area designation set forth in N.J.S.A. 40A:12A-5c and that, pursuant to N.J.S.A. 40A:12A-3, the Crescent Parcel is necessary for the effective redevelopment of the area; and

WHEREAS, after the conclusion of the public hearing described above, the Planning Board adopted a resolution recommending that the Township Committee designate the Crescent Parcel as an area in need of redevelopment pursuant to the Redevelopment Law; and

WHEREAS, the Township Committee agreed with the conclusion of the Planning Board that the Crescent Parcel satisfies the criterion for redevelopment area designation set forth in N.J.S.A. 40A:12A-5c and that, pursuant to N.J.S.A. 40A:12A-3, the Crescent Parcel is necessary for the effective redevelopment of the area, and the Township Committee found that such conclusion was supported by substantial evidence; and

WHEREAS, by Resolution adopted on December 12, 2012, the Township Committee designated the Crescent Parcel as an area in need of redevelopment, pursuant to N.J.S.A. 40A:12A-6, and authorized and directed the Planning Board to prepare an amendment to the FMC Site Redevelopment Plan to include the Crescent Parcel therein; and

WHEREAS, the Planning Board caused the Planning Consultant to prepare an amendment to the FMC Site Redevelopment Plan to incorporate the Crescent Parcel therein, as well as to prepare amendments to modify the permitted uses in said area and bulk standards applicable thereto; and

WHEREAS, the Planning Consultant prepared an amended redevelopment plan for the FMC Site and the Crescent Parcel, now known, collectively, as the "Princeton HealthCare System at Plainsboro Site", entitled "Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site", dated January 2013 (the "Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site"); and

WHEREAS, the Planning Consultant reviewed drafts of the proposed Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site and/or conceptual land use plans for the area with the Township's Master Plan Subcommittee on June 19, 2012, October 1, 2012, November 5, 2012, December 4, 2012 and January 8, 2013; and

WHEREAS, the Planning Consultant presented the Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site to the planning Board at its regularly scheduled meeting on January 22, 2013; and

WHEREAS, by resolution adopted at its January 22, 2012 meeting, the Planning Board recommended that the Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site be adopted as presented on the date thereof; and

WHEREAS, the Township Committee hereby finds that the Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site is in the best interests of the Township and now desires to adopt same; provided, however, the Township Committee desires to modify Section 9.8 of the proposed Amended Redevelopment Plan for the Princeton HealthCare System at Plains-

boro Site in order to accommodate a potential change in law applicable to the issue addressed by that section.

NOW, THEREFORE, BE IT ORDAINED by the Township Committee of the Township of Plainsboro as follows:

1. The aforementioned recitals are incorporated herein as though fully set forth at length.

2. The Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site, a copy of which is on file with the Township Clerk and incorporated herein by reference, is hereby approved pursuant to N.J.S.A. 40A:12A-7, except that the following language shall be inserted in the table set forth in Section 9.8 thereof in the rows entitled "ARC" and "ORC", immediately after the phrase "as defined by COAH or its successor agency/entity.": "If, however, at the time construction commences, State law or regulation provides an exemption or an affordable housing set-aside of less than 10 percent, then such exemption or lower set-aside percentage shall apply."

3. The Planning Consultant is hereby directed to revise the proposed Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site to incorporate the aforementioned change therein.

4. If any part of this Ordinance shall be deemed invalid, such parts shall be severed and the invalidity thereby shall not affect the remaining parts of this Ordinance.

5. A copy of this Ordinance shall be available for public inspection at the offices of the Township Clerk.

6. This Ordinance shall take effect 20 days after the first publication thereof after final adoption, as provided by law.

[ROLL CALL AND CERTIFICATION ON NEXT PAGE]

Introduced at a Meeting of the Township Committee of the Township of Plainsboro on February 27, 2013 and published on March 7, 2013. Adopted at a Meeting held on March 13, 2013 and published on March 16, 2013.

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1.0 INTRODUCTION

1.1 PURPOSE

The amended Redevelopment Plan set forth herein describes the vision and sets standards for the continuing redevelopment of the Princeton HealthCare System at Plainsboro site located on Route 1 between the Millstone River and Scudder's Mill Road in the Township of Plainsboro, New Jersey, which was designated as an Area in Need of Redevelopment on September 10, 2007¹. The boundaries of the Redevelopment Area were expanded to include an ±11 acre parcel, known as the "crescent parcel," on December 12, 2012.² (Figure 1 shows the location of the Redevelopment Area.)

Since the adoption of the original Redevelopment Plan on January 23, 2008, significant progress has been made in achieving the Township's overall vision for the area. The Merwick Care Center, a skilled nursing facility and dialysis center, was completed in 2010.³ The University Medical Center of Princeton at Plainsboro, a hospital and medical offices, was constructed and opened in 2012. Meanwhile, the former FMC buildings that existed on the site of the Public Park have all been demolished and the improvements for the public park are in place. This amended Redevelopment Plan reflects the development completed to date within the Redevelopment Area and sets forth a detailed regulatory framework for the development of the remainder of the Redevelopment Area in accordance with the Township's vision for the area.

1.2 SCOPE

The amended Redevelopment Plan first describes the Redevelopment Area, including the project boundaries and surrounding uses. The Redevelopment Area and the amended Redevelopment Plan's relationship to the intent and purpose of the Plainsboro Master Plan and Zoning Ordinance are then examined. Next the amended Redevelopment Plan describes the goals and objectives of redevelopment. The amended Redevelopment Area Land Use Plan sets forth the uses and area, yard and building standards for the redeveloped buildings and structures within the Redevelopment Area. The amended Redevelopment Area Circulation and Open Space Plan provides the required circulation system and open spaces within the Redevelopment Area. The proposed new bikeways and walkways to be constructed and the parks and other public spaces to be set aside as a consequence of the site's redevelopment are then described in detail. Redevelopment Area guidelines for high-quality development that will complement the existing development along the Route 1 corridor are also provided. Some plan elements are fixed, such as the location of certain new roads and open space to be set aside. Other standards allow some degree of flexibility, such as the location of buildings, parking areas, driveways and walkways via the bulk regulations and design guidelines set

¹ The original Redevelopment Plan was entitled "Redevelopment Plan for the FMC Corporation Site", prepared by Phillips Preiss Shapiro Associates, Inc. dated January 23, 2008.

² See Area in Need of Redevelopment Investigation, Block 1701, Lots 1 and 2, prepared by Phillips Preiss Grygiel LLC, adopted December 12, 2012.

³ The dialysis center is, however, not operational at this point in time.

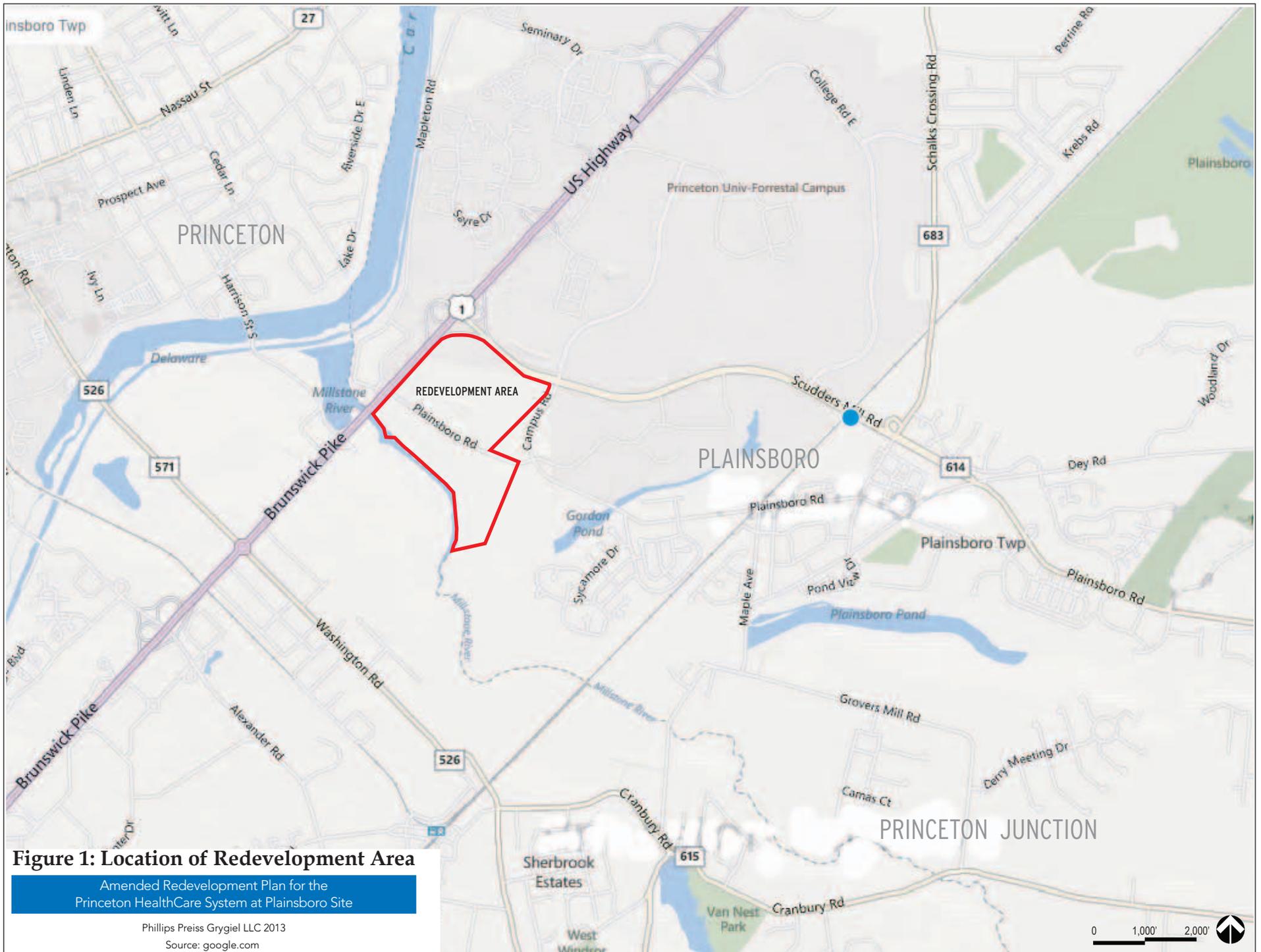


Figure 1: Location of Redevelopment Area

Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: google.com



forth in the Plan. These guidelines and standards form the framework in which creative development proposals which meet the plan's goals and objectives can be achieved.

1.3 PROCESS

The Township of Plainsboro's Planning Board, primarily through the Master Plan Subcommittee, had for a number of years prior to the designation of the Princeton HealthCare System (PHCS) at Plainsboro site (formerly named the "FMC Corporation site") as an "Area in Need of Redevelopment" sought to develop a plan for the site to be redeveloped with more productive land uses. A number of proposed amendments to the land use designation of the PHCS at Plainsboro site within the Township Master Plan were prepared as a prelude to amending the Zone Plan for the Township. Analyses of the property's potential reuse under a number of different zoning scenarios were considered. Beginning in 2006, the focus of discussion centered on its possible redevelopment as a hospital/medical office complex, as well as related residential health care uses and general office development.

The design and land use concepts of the original Redevelopment Plan have as their basis a series of four (4) public workshops conducted by the Plainsboro Master Plan Subcommittee in 2007, including: a meeting focusing on the concept plan and relationship to the Plainsboro Master Plan; a meeting focusing on the concept plan and relationship to the Plainsboro Master Plan; a meeting focusing on the Redevelopment Area Land Use/Bulk and Area Standards and on the Redevelopment Area Circulation and Open Space Plan; a meeting focusing on Redevelopment Area Design Guidelines and Standards; and a final workshop in which the draft Redevelopment Plan was reviewed in its entirety. Since the adoption of the original Redevelopment Plan in 2008, a series of meetings have been held with the designated redeveloper, Princeton HealthCare System, and its representatives to review the redeveloper's proposed conceptual plans for future phases of the overall project. Based on these meetings, it was determined by the Township that revisions to the original plan were necessary in order to achieve its overall vision for the amended Redevelopment Area.

1.4. DESCRIPTION OF THE REDEVELOPMENT AREA BOUNDARIES

On June 13, 2007 the Township Committee of the Township of Plainsboro determined the PHCS at Plainsboro site to potentially be an Area in the Need of Redevelopment pursuant to New Jersey's Local Housing and Redevelopment Law (NJSA 40:12A et seq.), and authorized the Plainsboro Planning Board to undertake an investigation and perform analyses to determine whether the PHCS at Plainsboro site met the criteria set forth therein. The Planning Board then authorized Phillips Preiss Shapiro Associates Inc. (PPSA), a planning and real estate consultant, to conduct the area in need of redevelopment investigation of the PHCS at Plainsboro site. Following completion and submission of the study in July 2007, and upon the consideration and recommendation of the Planning Board, the Township Committee declared the PHCS at Plainsboro to be an Area in Need of Redevelopment on September 10, 2007. As noted, the original Redevelopment Plan was adopted on January 23, 2008. Block 1701, Lots 1 and 2 were added to the Redevelopment Area by an amendment to the Area in Need of redevelopment designation on December 12, 2012. On the same day the Township

Committee directed the Planning Board to formulate an Amended Redevelopment Plan for the Redevelopment Area.

According to the official tax maps of the Township, the Redevelopment Area encompasses approximately 171 acres and includes one (1) property assemblage, consisting of eight (8) tax lots on portions of three (3) tax blocks. As shown in Table 1, the tax blocks include Block 1701, Lots 1, 2, 3.01 and 4.01; Block 1703, Lots 1.011, 2.02, and 3.01; and Block 1704, Lot 1. The lots range in size from ± 0.25 acres to ± 49 acres. At its widest point, the Redevelopment Area is $\pm 2,562.3$ feet wide and varies in length from $\pm 1,820$ feet on the west side to $\pm 3,896$ feet on the east side. The property has $\pm 1,820$ feet of frontage on Route 1, 350 feet of frontage on Scudders Mill Road, and $\pm 2.893.3$ feet of frontage on Plainsboro Road.

Table 1: Tax Lots Located within the Redevelopment Area

Block	Lot	Area
Block 1701	Lot 1	9.434 acres
	Lot 2	1.902 acres
	Lot 3.01	49.421 acres
	Lot 4.01	33.154 acres
Block 1703	Lot 1.02	32.141 acres
	Lot 2.02	6.396 acres
	Lot 3.01	38.215 acres
Block 1704	Lot 1	0.249 acres
TOTAL		170.912 acres

The Redevelopment Area is bounded generally by U.S. Route 1 to the west; the municipal border of the Township of West Windsor and the Millstone River to the south; Campus Road and state-owned lands to the east; and Scudders Mill Road to the north. Plainsboro Road bisects the property, creating a northern and southern portion of the former FMC property. The Redevelopment Area boundaries are shown on Figure 2.

In terms of its locational context, the Redevelopment Area is located within a portion of the Township characterized primarily by large corporate office campuses. To the north, across Scudders Mill Road, is the Princeton Forrestal Center, a corporate research center owned by Princeton University. To the northeast of the Redevelopment Area is a corporate campus for Bristol-Myers Squibb, a global pharmaceutical company. In the area north of Plainsboro Road and east of the Redevelopment Area is the former Merrill Lynch corporate campus. Located to the south of Plainsboro Road and east of the Redevelopment Area is the corporate headquarters of Firmenich Incorporated. To the west, across Route 1, is a commercial area which includes a Ruby Tuesday restaurant, as well as a Marriott Courtyard hotel and a Homewood Suites hotel.

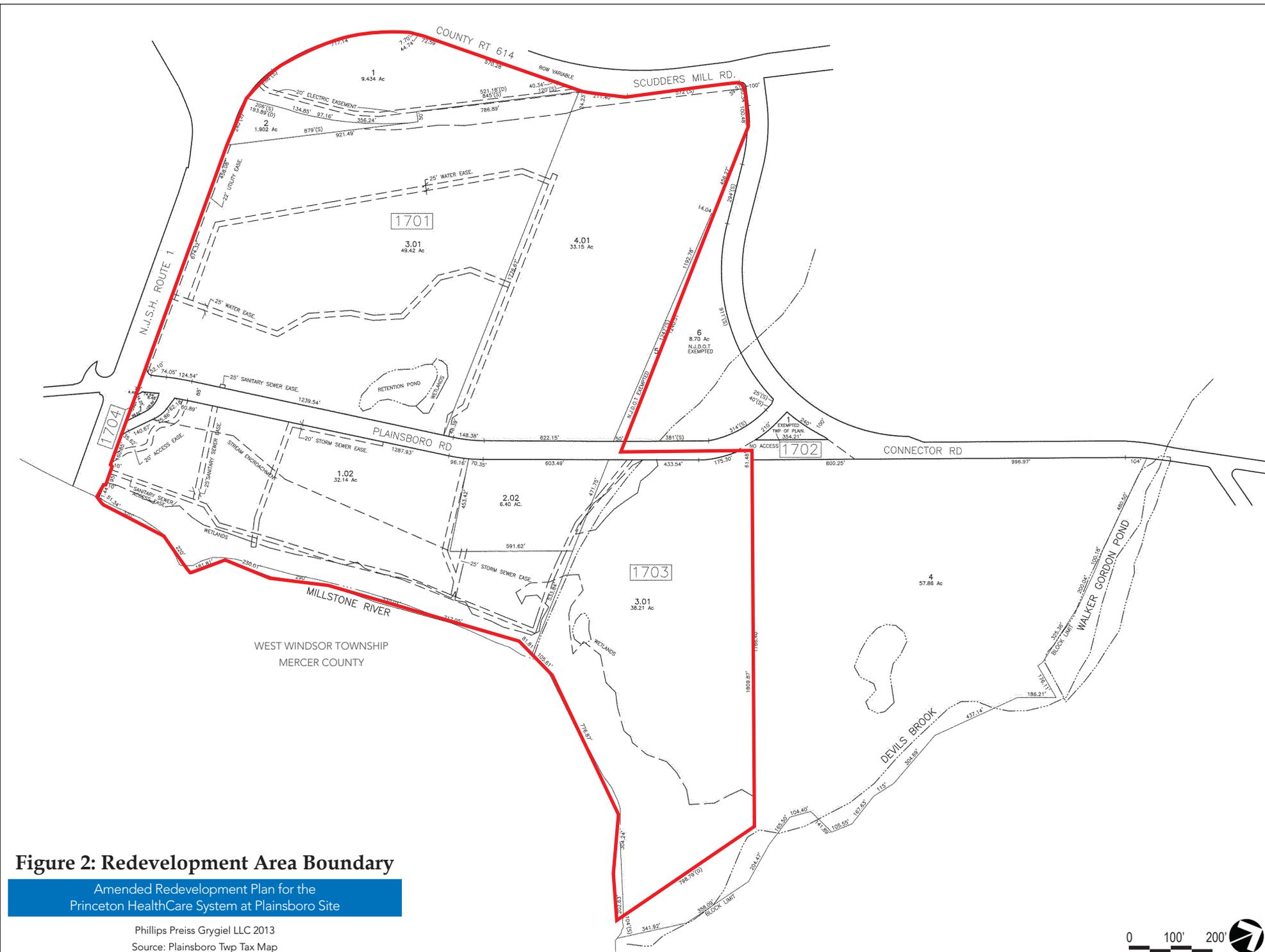


Figure 2: Redevelopment Area Boundary
 Amended Redevelopment Plan for the
 Princeton HealthCare System at Plainsboro Site
 Phillips Preiss Grygiel LLC 2013
 Source: Plainsboro Twp Tax Map

1.5 EXISTING CONDITIONS IN THE REDEVELOPMENT AREA

The Redevelopment Area consists of eight (8) tax lots on three (3) tax blocks, all of which are currently under common ownership by Princeton HealthCare System except for the Merwick Care Center, which is owned by Windsor Health Care. It is irregularly shaped and located in the southwestern portion of the Township along the Millstone River, which also serves as the northern border of West Windsor Township.

Since the adoption of the original Redevelopment Plan in 2008, all of the buildings formerly utilized by the FMC Corporation have been demolished and substantial redevelopment has occurred (see Figure 3). The first phase of the University Medical Center of Princeton at Plainsboro opened in 2012; the Merwick Care Center, a skilled nursing facility, opened in 2010; and the public park has just been completed. The remainder of the Redevelopment Area remains vacant and in need of redevelopment.

1.6 ENVIRONMENTAL CONSTRAINTS

1.6.1 Wetlands and Floodplains

Both wetlands and floodplains are present within the Redevelopment Area, which limits the extent of redevelopment that is possible. Figure 4, Environmental Constraints, indicates the location and boundaries of the wetlands and floodplains.

The predominant areas of constraint are associated with the portion of the Redevelopment Area located along the Millstone River south of Plainsboro Road. Figure 4 depicts the wetland boundary, whose depth varies from approximately 50 feet at its closest point to the River to over 300 feet in other portions, but averages about 100 feet in depth. Since this wetland is of intermediate value, a buffer transition area of 50 feet is required adjacent to the wetland.

In addition, a small strip of ordinary wetlands is located along the edge of Scudders Mill Road in the northeast corner of the Redevelopment Area. Ordinary wetlands do not have a transitional buffer requirement.

Figure 4 also depicts the 100-year floodplain and the New Jersey flood hazard area limit, also associated with the Millstone River. Also shown on Figure 4 is the Delaware and Raritan Canal Commission (DRCC) buffer boundary line, which is located another 100 feet inland from the 100-year floodplain line. Virtually the entire portion of the Redevelopment Area south of Plainsboro Road, except for an area located opposite the new retention basin of the University Medical Center of Princeton at Plainsboro, is within the New Jersey flood hazard area limit.

With the exception of ordinary wetlands, consistent with the regulations of the State of New Jersey, development within or disturbance and/or filling of the wetland areas is strictly limited. Permits and approvals for development within these areas are extremely difficult to come by, and only permitted



Figure 3: Existing Conditions within the Redevelopment Area

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: bing.com

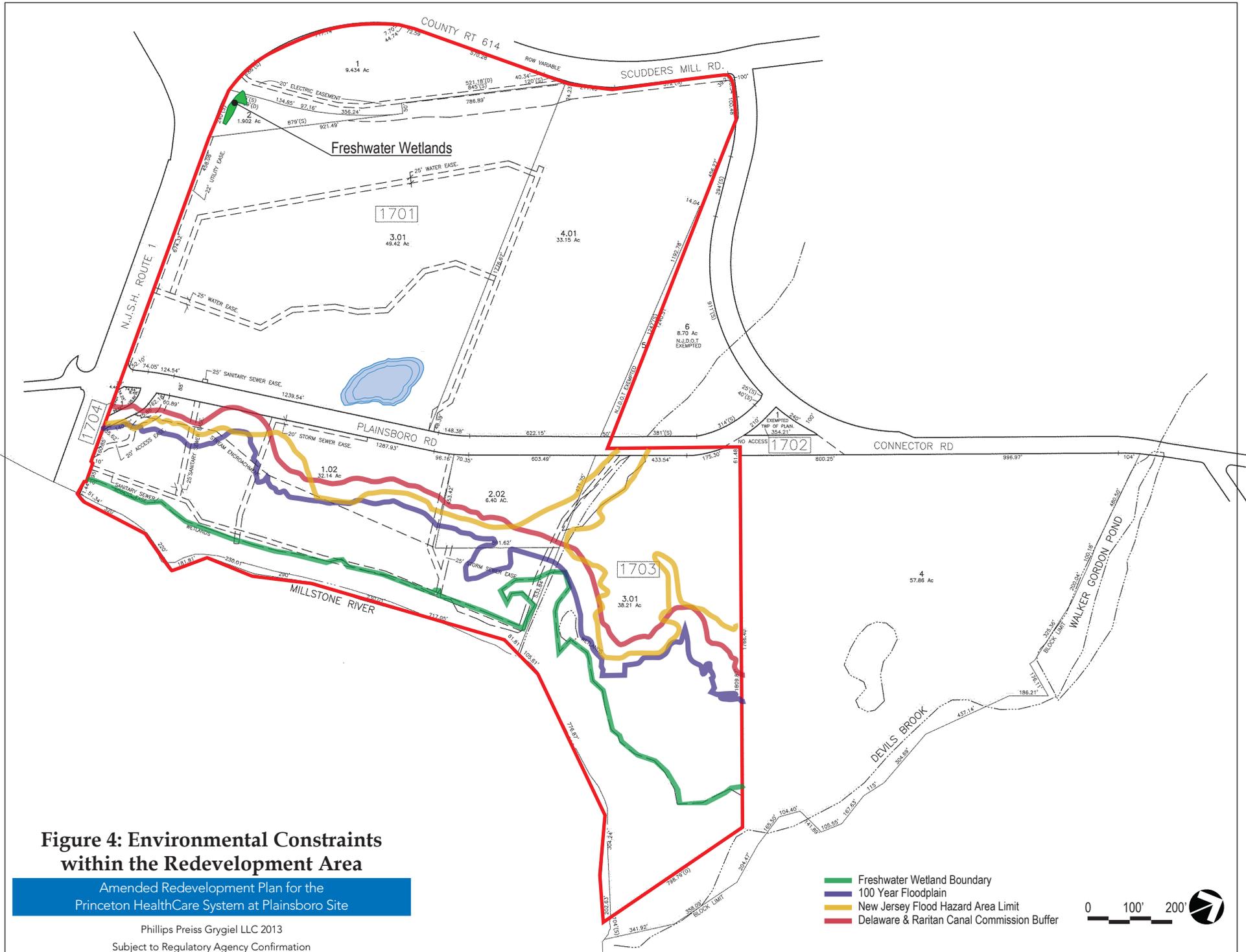


Figure 4: Environmental Constraints within the Redevelopment Area

Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013

Subject to Regulatory Agency Confirmation

- Freshwater Wetland Boundary
- 100 Year Floodplain
- New Jersey Flood Hazard Area Limit
- Delaware & Raritan Canal Commission Buffer



under exceptional circumstances. Development and filling of floodplain areas are also limited by regulation.

1.6.2 Environmental Assessment

Given the Redevelopment Area's history of use, a possibility existed at the time the area was originally deemed "in need of redevelopment" that some form of environmental contamination existed on the PHCS at Plainsboro site. Since the transfer of the property comprising the Redevelopment Area was subject to the provisions of the Industrial Site Recovery Act (ISRA), the former owner and seller of the property, FMC Corporation, was responsible for compliance with ISRA and the regulations promulgated thereunder. FMC conducted assessment and site investigation under the supervision of the New Jersey Department of Environmental Protection (NJDEP). No conditions were identified that presented a significant environmental hazard.

1.7 VISION

The original PHCS at Plainsboro Site Redevelopment Plan envisioned the development of a world class health care-oriented mixed-use complex anchored by the University Medical Center of Princeton at Plainsboro (UMCPP), along with related medical facilities and offices, residential health care facilities and general offices. Since the original Redevelopment Plan was adopted three significant projects have been undertaken. The Merwick Care Center, a skilled nursing facility, opened in 2010. The hospital and medical offices of the UMCPP opened in 2012. More recently in 2012, improvements to the Public Park were completed and the park has been opened to the public. The vision now is focused on broadening the medical facilities to be provided within the Redevelopment Area, complemented by residential health care facilities. More specifically, the vision for the amended Redevelopment Area includes a pediatric care/medical-office facility, a day care center, general offices, and age-restricted and other residential health care facilities. The entire Redevelopment Plan Area is planned as a single, comprehensive and integrated development, with coordinated and shared utilities, circulation and open space, although each portion of the districts of the Redevelopment Area that remain to be developed, may be developed by different entities on their own subdivided parcel, over time.

An overarching goal of this amended Redevelopment Plan is to create a campus environment which generates synergy between the different components. Consistency and excellence in design, materials, quality of the built environment (including parking facilities and accessory buildings and facilities) and the non-built environment is envisioned, creating a striking visual environment, and complementing the newly developed world-class health care facilities, which serves the Township and wider region. Walking paths, open spaces, water features, a green corridor and other common design elements will ensure that physical linkages between the various uses are provided.

This amended Redevelopment Plan encourages a relationship between health and wellness and environmental sustainability in both the indoor and outdoor environments. The University Medical Center of Princeton at Plainsboro has set a high standard for environmental sustainability in building

design by including numerous design features, such as a high-performance curtain wall system, solar panels, a co-generation facility and water-saving fixtures, which create significant reductions in energy and water usage. The amended Plan encourages similar sustainable design elements for all future phases of development.

In addition, the amended Plan is based on the idea that creating an attractive and engaging outdoor environment is beneficial to the health and well-being of not just patients, but also doctors, employees, visitors and other members of the community. Consistent with the promotion of healthy lifestyle habits for preventative care, employees, patients and visitors will be able to take long walks through bucolic settings with attractive vistas and scenery. Healing gardens, rain gardens and common open space areas will continue to be included within each district to help provide respite and recreational space for the benefit of the entire community.

A further opportunity is provided by the regional setting of the Redevelopment Area. In addition to the University Medical Center of Princeton at Plainsboro, Princeton University, Princeton Forrestal Center and other local institutions provide opportunities for partnerships and connections which could extend well beyond the physical connections described in this Plan. Redevelopers will be encouraged to tap into the assets and services provided by local institutions and other members of the Plainsboro community.

2.0 RELATIONSHIP TO LOCAL OBJECTIVES

2.1 RELATION TO INTENT AND PURPOSE OF MASTER PLAN

The most recent comprehensive Master Plan for the Township of Plainsboro was adopted in 2009, which occurred after the adoption of the original Redevelopment Plan in 2008.⁴ Accordingly, it is necessary to evaluate this amended Redevelopment Plan in relation to the intent and purpose of the 2009 Township of Plainsboro Master Plan.

The Plainsboro Master Plan describes the Township's transformation in the last several decades from an almost exclusively rural farming community to a more suburban, developed community with a wide variety of uses. According to the Master Plan, the Route 1 corridor, of which the Redevelopment Area is a significant part in Plainsboro, contains major office research, shopping, and medium-density residential developments.

With respect to the Redevelopment Area itself, the Master Plan Land Use Element describes the Redevelopment Plan adopted in 2008 and indicates that no further zoning changes are necessary for the area (see Section H of the Land Use Plan Element, pp. 59-66).

The Township has adopted a Redevelopment Plan for the FMC site which is located in the I-100 Limited Industrial Zone...As a result of Redevelopment Plan adoption it will not be necessary to change any of the zoning or land development codes to address this development plan.

The **Land Use Element** of the Plainsboro Master Plan designates the subject property for I-100 Limited Industrial use and recognizes on the Existing Land Uses map that the parcel is currently used for industrial purposes. In reference to the subject property, the Master Plan states:

Along that portion of Plainsboro Road that lies west of the railroad and adjacent to the PMUD zone is located the largest area of Plainsboro's second research and industrial development zone, which includes the unique and extensive research, industrial and office campus facilities of FMC and Firmenich.

The Master Plan further recites the land use regulatory framework for each of the districts contained in the original Redevelopment Area, including the Hospital Medical Office Complex (HMOC), the Skilled Nursing Facility (SNF), the Continuing Care Retirement Community (CCRC), the General Office Research Complex (GORC) and the Public Park (PP). Several renderings of the University Medical Center of Princeton at Plainsboro are included, as well as a copy of the Land Use Plan con-

⁴ The Township also adopted one minor revision to the 2009 Master Plan in 2010, focusing on a single property, which had no impact on overall policies or goals of the overall master plan, and none on the Redevelopment Area.

tained in the original redevelopment Plan. While this Amendment contains several modifications to the sub-districts described in the 2009 Master Plan, the overall intent and vision for the land uses to be developed within the Redevelopment Area remains unchanged, and as such, this amended Redevelopment Plan is generally consistent with the Land Use Element of the Township's 2009 Master Plan.

The Economic Element of the Master Plan sets forth several economic development objectives which are advanced by this Redevelopment Plan, including the following:

- Encourage development of industrial, office, research, commercial, and service uses, selected and regulated so as to preclude land use incompatibilities and in an amount that would increase the tax base which supports the local government and the public school system without disturbing the fragile residential-agricultural-open space balance in the rest of the Township.
- Limit office-research and industrial uses to the areas already set aside for such uses by reason of accessibility to transportation.
- Use the leverage offered by Plainsboro's unique existing and prospective character to continue to attract office-research development of the highest quality.

The Economic Plan Element further identifies the Redevelopment Area as one of "the other areas of the Township that could experience a substantial amount of change involving new development." It also supports a cooperative effort with the University Medical Center of Princeton at Plainsboro to redevelop the area:

The Planning Board passed a resolution with a recommendation for the Township Committee to adopt the Redevelopment Plan for the FMC Corporation Site which allows development of the properties for a new Princeton Medical Center campus, based on a series of new planning goals and objectives for this area, intended to lead toward the establishment of a high quality coordinated and comprehensive plan of development. As a result, the Township decided to work in a cooperative effort with FMC and the Princeton Medical Center to explore development for this area.

In summary, the Economic Element clearly anticipates and supports the development of a health care-oriented medical/office complex within the Redevelopment Area. The Township continues to work closely with the designated redeveloper to develop the Redevelopment Area in accordance with Plainsboro's vision for the site. Thus, the Amended Redevelopment Plan is consistent with the objectives and intent of the Economic Element.

The Circulation Element of the Master Plan contains an extensive description of the trip reduction and transit strategies that will be undertaken within the Redevelopment Area:

By way of example, the Princeton HealthCare System has offered to implement trip reduction and transit strategies within the FMC Redevelopment Area. The University Medical Center of Princeton at Plainsboro (UMCPP) will explore instituting an employee shuttle from Princeton to the Plainsboro facility, if no direct NJ Transit Link is established, as well as enhancing existing shuttle programs, such as TrainLink (connecting the Princeton Junction Train Station to the Health Campus). The Medical Center will be working with the other development partners on the site (Medical Office Building(s), Retirement Community, Skilled Nursing Facility, Fitness Center) to establish campus wide shuttles linking the campus with the Plainsboro Town and Village Centers and perhaps other locations in the area depending upon the demand. The Medical Center will market transit opportunities with continuous employee communications, including transit information fairs, and incentives (i.e. prizes for mass transit users).

The Medical Center also will institute a Ride Share program in collaboration with the Greater Mercer Transportation Management Association (GMTMA). According to the GMTMA, the simplest of ride-share programs can reduce vehicle commute trips to specific worksites by five (5) to fifteen (15) percent. To get the percent of use as high as possible, the Medical Center will encourage employees to enroll in the State's "Car Pooling Makes \$ense" program which gives gas cards to individuals carpooling a certain percentage of their work week. The car pool program also would include a guaranteed emergency ride home provision.

NJ Transit is planning on serving the site by Bus Rapid Transit (BRT). NJ Transit's BRT study Executive Summary noted that the BRT would increase the percentage of work trips using transit from a range of two (2) to four (4) percent to a range of five (5) to nine (9) percent in the core study area (West Windsor, Plainsboro, Princeton Township and Princeton Borough). It should also be noted that the Master Plan mentions the possibility of including a Bus-Rapid-Transit (BRT) route through the Redevelopment Area at some point in the future.

Since all of the above strategies are promoted by the Redevelopment Plan, it is substantially consistent with the Circulation objectives set forth in the Master Plan.

Along that portion of Plainsboro Road that lies west of the railroad is located Plainsboro's I-100 Zone which includes the unique and extensive research industrial and office campus facilities of FMC and Firmenich. An additional 1.2 million square feet could be constructed.

While the portion of the I-100 Zone in which the subject site is located had been appropriate for research, industrial, and office campuses in the past, this Plan underscores the belief that in terms of gateway function, the existing conditions and underutilization of the PHCS at Plainsboro site, the hospital and related office and residential uses would provide the community with substantially greater economic benefit than what the current Master Plan envisions.

The **Open Space and Recreation Element** sets forth several goals, including the following:

- Maintain and protect Plainsboro's rural heritage and open spaces.
- Conserve and protect as many environmentally sensitive areas as possible.
- Maintain preserved areas and make them accessible to all members of the public, where appropriate.

Several policies are put forth, including the following:

- Pursue land set-asides—Through various tools, secure lands either under terms of preservation or public control. This results in land being protected from development and left available for passive or active recreation.
- Enhance the interaction of Township residents with the environment by acquiring lands for public park use, which also coincidentally preserves the most important natural resource of the Township.
- Extend park and recreation open space corridors to permanently protect significant portions of the Township's natural features.

In terms of open space and recreation, the Redevelopment Plan envisions that a significant portion of the Redevelopment Area south of Plainsboro Road, located adjacent to the Millstone River and incorporating areas of valuable natural resources and environmental constraints for development, will be set aside as a publicly-accessible passive open space for use by all Township residents. Furthermore, even on parcels accommodating development—including the hospital, residential health care facilities and offices—open space is to be set aside not only for visual relief, but also as integral to the function, use and enjoyment of the redeveloped environment by patients, employees and residents. Moreover, integration and accessibility via interconnected walkways and bikeways are also envisioned in the Redevelopment Plan, consistent with the objectives of the Plainsboro Master Plan.

The Housing Element of the Master Plan describes the affordable units which are anticipated to be accommodated within the Redevelopment Area, as follows:

Within this redevelopment site, nine assisted living Medical Waiver bedrooms and 82 independent senior living apartments are proposed to be integrated into the assisted living and independent apartment complexes. Both the nine assisted living apartments and the 82 senior apartments comprise the affordable component.

The Amended Redevelopment Plan does not change the underlying policy that the affordable housing obligation resulting from residential development within the Redevelopment Area be addressed on site. How that obligation will be addressed, however, has been altered as a result of these amendments and is specifically described in Section 9.8 of this Plan.

Finally, the Conservation Element of the Master Plan cites the green building and sustainable design aspects of the University Medical Center of Princeton at Plainsboro as a model for other developments within the Township.

The new Medical Center is focused on environmental design and includes the incorporation of many elements that represent time tested approaches to sustainable design. Major sustainable design elements and strategies include: an overall east-west building orientation that facilitates control of the sun and takes advantage of efficiencies in heating and lighting, solar shades on the south elevation that reduce heat gain in the summer, site stormwater recharge, a heat recovery system for the mechanical systems, and use of recycled construction materials and landscape design consisting of native species of plants requiring minimal irrigation. The hospital will be built according to LEED standards and it should achieve the equivalent of a silver level of certification. This will not only make the facility more energy efficient and create a better healing environment for patients, but will also have significant impact in the community. Among the many sustainable aspects to the site, UMCP is looking into taking the lead on a power generation initiative that would bring energy efficiency and cost savings not only to the Hospital but also to community partners on and off the Campus.

In summary, the Redevelopment Plan is consistent with the intent and purpose of the 2009 Township of Plainsboro Master Plan.

2.2 RELATION TO INTENT AND PURPOSE OF ZONING ORDINANCE

The Plainsboro Land Use Element designates the Redevelopment Area as the “Princeton Health Care System Redevelopment Area.” While certain modifications to the Redevelopment Area districts are effectuated by this Amendment, the overall vision for a healthcare-oriented mixed-use complex to be developed within the Redevelopment Area remains unchanged. As such, it is consistent with the Township’s Master Plan (see Figure 5).

2.3 REDEVELOPMENT GOALS AND OBJECTIVES

The amended Redevelopment Plan is intended to promote the specific Goals and Objectives listed below. These are the same goals and objectives of the original Redevelopment Plan.

1. To provide for and encourage future redevelopment of the site in a manner that is truly coordinated, comprehensive, synergistic, and sustainable.
2. To provide for a variety of land uses that will eliminate unproductive uses, promote economic development, growth opportunities, and open space preservation, provide public spaces, and uses or activities that serve the needs of the community.
3. To provide a land use mix of complementary uses, consisting of medical and residential health care facilities, as well as medical and general offices and independent senior residential uses.

4. To secure a proper circulation network that will utilize existing roadways where possible, minimize traffic movement into the Village, encourage orderly movement vis-à-vis wayfinding signage, include attractively landscaped roadways, particularly a boulevard serving as a central spine extending through the principal development site, and to promote multi-modal forms of transportation, including a rapid bus transit service, pedestrian pathways, and bicycle access.
5. To create an aesthetically-pleasing gateway into the Township.
6. To encourage high-quality development inclusive of an interesting and varied architectural design and materials, and an overall high level of compatibility and coherence within the overall design.

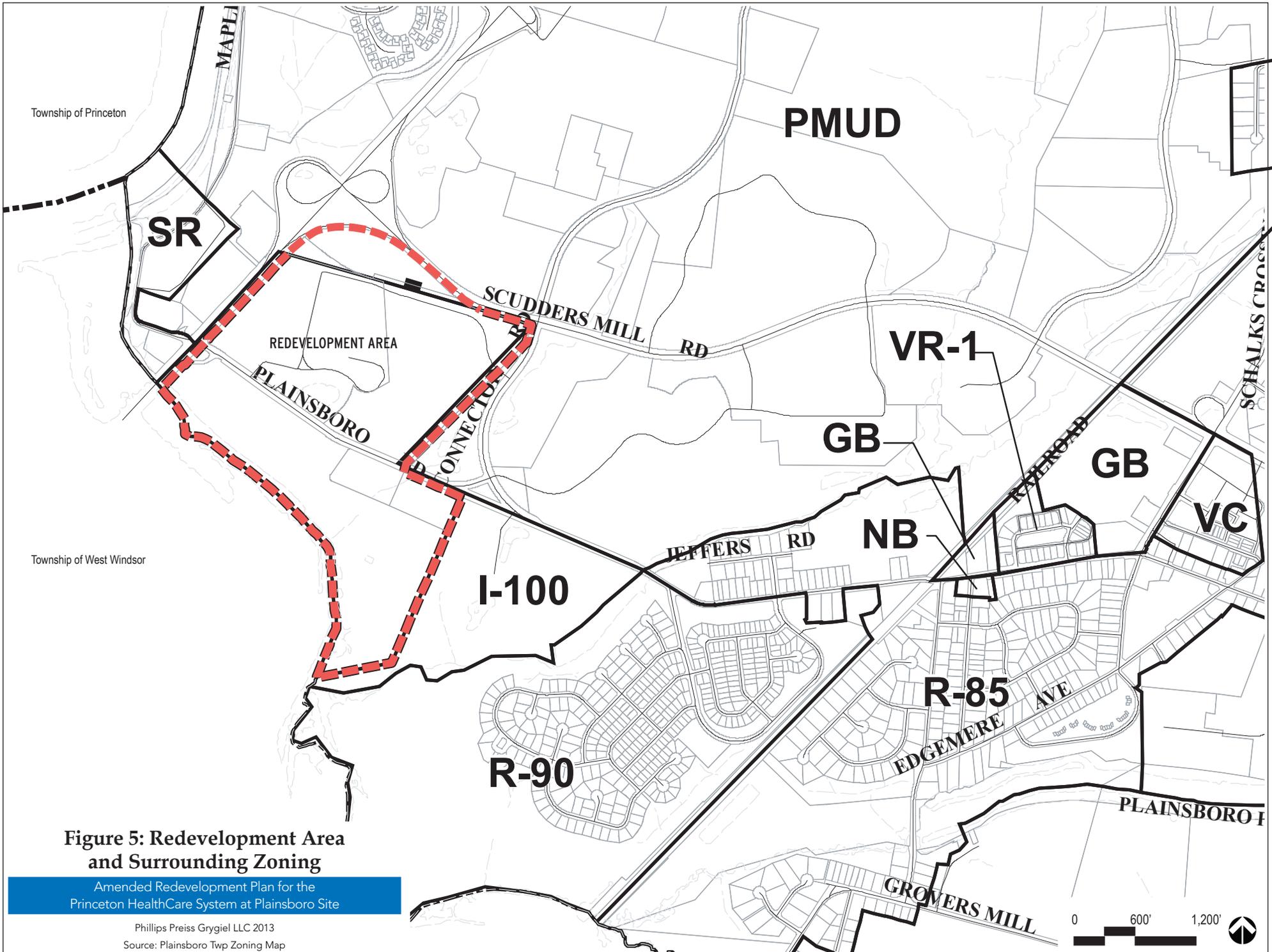


Figure 5: Redevelopment Area and Surrounding Zoning

Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
 Source: Plainsboro Twp Zoning Map

7. To promote environmental sensitivity through limiting impervious surfaces, by encouraging parking structures and allowing taller buildings, by encouraging NJDEP best management practices, and requiring open space throughout the Redevelopment Area, and curtailing development or actually removing structures which are incompatible and potentially damaging to the 100-year floodplain of the Millstone River.
8. To promote passive recreational activities in floodplain areas, and to the extent permissible, in wetland and wetland-transition areas.
9. To encourage a world-class multifaceted development that will serve as a destination to both Township residents and the region.
10. To advance the goals of sound state, regional, and local planning objectives by maximizing the preservation of open space, clustering development within the Redevelopment Area where suitable, and encouraging a mixed-use pattern of development that can be readily served by both public transportation and the private automobile.
11. To maintain the historic tree-lined environment of Plainsboro Road.

3.0 AMENDED REDEVELOPMENT AREA LAND USE PLAN

The amended Redevelopment Plan provides eight (8) separate but interrelated districts within the Redevelopment Area (Figure 6 shows the boundaries of each district). This chapter sets forth the use and bulk regulations for the overall Redevelopment Area and each of its districts. Redevelopment pursuant to this amended Redevelopment Plan includes three districts for which redevelopment is substantially complete—a Hospital Medical Office Complex (HMOC) pursuant to Section 3.2; a Skilled Nursing Facility (SNF) pursuant to Section 3.3; the Public Park (PP) pursuant to Section 3.4; and five districts which are yet to be undertaken—the Pediatric Medical Office (PMO) pursuant to Section 3.5; a Day Care Complex (DCC) pursuant to Section 3.6; an Adult Retirement Community (ARC) pursuant to Section 3.7; an Office/Residential Complex (ORC) pursuant to Section 3.8; and a Hospital Auxiliary District (HAD) pursuant to Section 3.9.

3.1 Overall Plan

An amended General Land Use Plan for the development of the Redevelopment Area as a whole shall be submitted in accordance with the requirements of Section 9.2.1 (Supplementary Submission Requirements), and in accordance with the land use and area, yard and building requirements set forth in this amended Redevelopment Area Land Use Plan, the amended Redevelopment Area Circulation and Open Space Plan (Chapter 4), and the design requirements set forth in Chapter 5, amended Redevelopment Plan Design Guidelines and Standards. For illustrative purposes only, a conceptual plan for development in accordance with this amended Redevelopment Plan is shown in Figure 16.

The cumulative development within the entire Redevelopment Area shall not exceed the following:

Standard	Requirement
Floor Area Ratio (FAR)	0.35
Impervious Surface Coverage	55%

3.2 The Hospital/Medical Office Complex (HMOC)

The area shown on the Redevelopment Area Land Use Plan as Hospital Medical Office Complex (HMOC) permits the continued development of that portion of the Redevelopment Area as a hospital-anchored medical office complex. The HMOC shall continue to be developed as a single, integrated and comprehensively designed and planned hospital/medical office campus, designed to allow for safe and convenient access by all forms of transportation—vehicular and truck circulation, bus rapid transit, pedestrian and bicycle circulation—interspersed with gardens and landscaped areas and visually and functionally connected to the other components of the Redevelopment Area. The hospital and medical offices shall continue to be developed with state of the art facilities, incor-



**Figure 6: Redevelopment Area
Land Use Plan**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: PPG 2013



porating striking architectural features and sustainable building materials and design, and able to accommodate expansions and changes in health care technology and delivery well into the future.

Substantial implementation of the HMOC has already occurred since the adoption of the original Redevelopment Plan in 2008. Namely, the first phase of the University Medical Center of Princeton at Plainsboro opened in 2012. This consists of a 231-bed, 636,000 square foot hospital, a 48,000 square foot fitness center, 146,870 square feet of medical office space, a co-generation facility, accessory surface parking and stormwater management facilities. In addition, a solar canopy array system has been installed over a portion of the parking lot. The second phase of development within the HMOC is anticipated to include a two-story addition of approximately 321,000 square feet to the existing hospital building comprising 130 private rooms for patient beds and the enclosure of the rooftop mechanicals, and a ±93,000 square foot addition to the Medical Office Building.⁵

Other incidental expansions may also occur, e.g., expansion of the emergency department, the addition of vestibules or other building features, or expansion of the hospital building to house additional diagnostic and treatment equipment. These expansions could occur at any time and are not necessarily linked to any particular phase of the development.

- A. Medical Arts Pavilion: Structured parking shall be required to accommodate the parking needs of any expansion to the Medical Arts Pavilion.
- B. Additional floors on existing hospital building: Structured parking shall be required to accommodate the parking needs of any additional floors on the hospital building.
- C. Incidental expansions:
 - 1. Except as specifically set forth in A and B above, to address the incidental expansions referenced above, a total of 10,000 additional square feet of development may be added to the HMOC site without automatically triggering the need for structured parking. Each request for expansion shall be accompanied by a parking analysis demonstrating how much additional parking the proposed expansion will require. For purposes of this analysis, and any other parking ratios set forth in this amended Plan notwithstanding, the presumptive ratio shall be 5 spaces per 1,000 square feet of development. If the parking analysis shows that the expansion will result in a site-wide deficiency, the Planning Board in its discretion may either require that structured parking be provided, or approve an alternate parking solution, to be used only until a parking structure is built.
 - 2. Structured parking shall be required to accommodate the parking needs of any additional development on the HMOC site beyond the 10,000 square feet referenced in C.1 above.

⁵ Sometimes referred to as the “Medical Arts Pavilion.”

All future development of the HMOC, including the second phase, shall be in conformance with the following standards.

3.2.1 Permitted Uses in the HMOC

- A. Principal hospital and hospital-related uses, including:
 - 1. Patient rooms, for both inpatient and outpatient treatment and care.
 - 2. Hospital support facilities, such as medical laboratories, medical clinics, surgical facilities, diagnostic testing, physical therapy and inpatient pharmaceutical facilities.
 - 3. Hospital operations, including but not limited to food service, laundry facilities, house-keeping, security facilities, and maintenance and storage facilities for medical supplies and equipment.
 - 4. Offices for hospital administration and hospital employees.
 - 5. Educational, training and conference facilities.

- B. Medical offices, which may include diagnostic, treatment and medical procedure facilities for private physicians, other health care practitioners and their employees, whether or not having staff privileges at the hospital.

- C. Accessory uses and facilities, as follows:
 - 1. Overnight accommodations and eating facilities for on-duty hospital employees and medical residents.
 - 2. Overnight accommodations within the hospital for patients' families.
 - 3. Accessory retail facilities serving hospital employees, patients and visitors, including but not limited to dry cleaning (drop-shop only), barber and beauty shops, massage therapy/health spa, pharmaceutical and medical supply sales, branch banks, gift shops, newsstands, and florists.
 - 4. Employee services, such as credit unions.
 - 5. Cafeterias for visitors and employees.
 - 6. Fitness centers.
 - 7. Child care centers, subject to the minimum standards of the Plainsboro Zoning Ordinance § 101-65F.
 - 8. Chapels and places of worship.
 - 9. Off-street parking and loading facilities, including both surface parking and multi-level parking garages.⁶
 - 10. Transit facilities, including Bus Rapid Transit stops.
 - 11. Communication facilities, including radio communications and radio and satellite antennas.
 - 12. Outdoor and indoor gardens, open space and landscaped areas.
 - 13. Utility and heating, air conditioning and ventilating facilities, including cogeneration facilities.

⁶ The trigger for the construction of structured parking on the HMOC site is set forth in Sections 3.2 and 5.1.1 G of this amended Redevelopment Plan.

14. Solar canopy array or roof-mounted systems.
15. Other accessory uses customarily incidental to hospital facilities.

3.2.2 Area, yard and building requirements for the HMOC

Standard	Required/Allowed
Minimum lot area	40 acres
Minimum lot frontage	700 feet
Minimum building setbacks from perimeter lot lines	75 feet
Minimum setback of building to driveways and parking lots	5 feet
Minimum setback of parking areas from perimeter lot lines: - Route 1, Plainsboro Road and Punia Boulevard - Other lot lines	50 feet 25 feet
Minimum setback between buildings and parking garage	10 feet
Maximum building height (feet/stories)*	175 feet/10 stories
Maximum floor area ratio (FAR)	0.60
Maximum impervious coverage on lot	60%
Maximum permitted height of parking garage (levels/feet)	6 levels/75 feet

* Except parking garage

3.3 The Skilled Nursing Facility (SNF)

The area shown on the amended Redevelopment Area Land Use Plan as the Skilled Nursing Facility (SNF) permits the continued development of that portion of the Redevelopment Area as a skilled nursing facility. The SNF provides a broad array of short-term and long-term medical and health care services to patients suffering from chronic illness and disabilities, as well as those requiring long-term rehabilitation and care. The SNF shall continue to be developed as a single, self-supporting entity, but with functional and physical connections to other components of the Redevelopment Area, with particular orientation to and physical integration with the Public Park.

In 2008, site plan approval was granted to develop the Merwick Care Center, a skilled nursing facility operated by Windsor Healthcare Communities. The facility opened in 2010. The Merwick Care Center consists of a 200-bed, 107,030 square foot skilled nursing facility which provides sub-acute care, long-term care and also a dialysis center. The building itself is made up of a series of interconnected “pods.” As per the original site plan approval, 129 parking spaces were provided. Of these, 12 spaces were land-banked in the northwestern corner of the site for future use. The intensive rehabilitation unit within Merwick generated an unanticipated need for parking and, as a consequence, a parking shortage developed shortly after the opening of the facility. To accommodate the additional parking, the boundaries and size of the SNF district is being expanded. A rectangular-shaped portion of the Public Park, comprising approximately 1.5 acres, and located directly to the south of the originally designated SNF district, has been redesignated as part of the SNF district. In addition, this amended plan has revised the parking standards for the SNF (see Section 5.2.1).

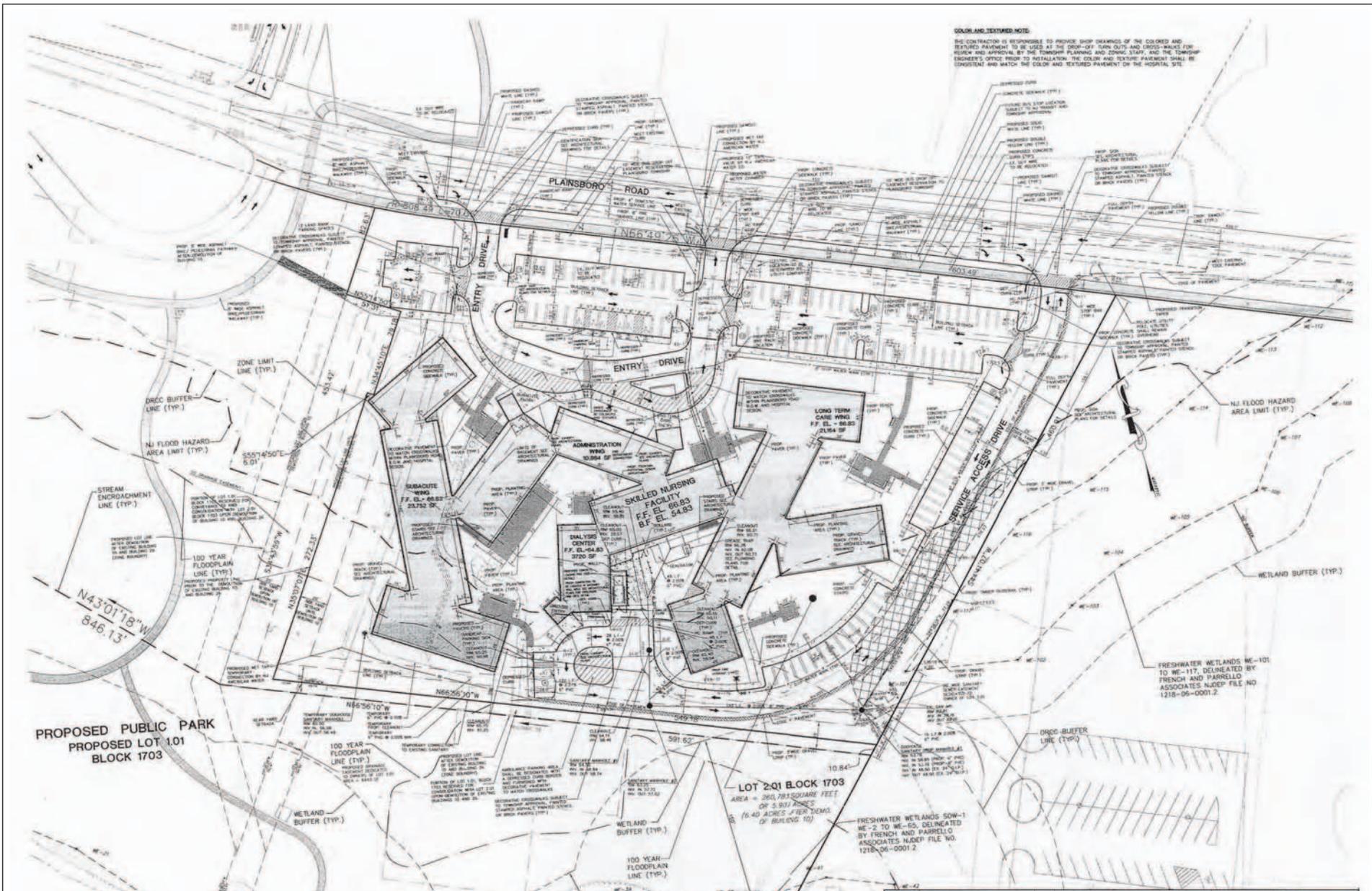


Figure 8: Approved Site Plan for Skilled Nursing Facility

Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013

Source: French & Parrello

<p>1. PREPARED BY: FRENCH & PARRELLO</p> <p>2. DATE: 08/14/13</p> <p>3. PROJECT: PRINCETON HEALTHCARE SYSTEM AT PLAINSBORO SITE</p> <p>4. SHEET: 1 OF 1</p> <p>5. SCALE: AS SHOWN</p> <p>6. DRAWN BY: [Signature]</p> <p>7. CHECKED BY: [Signature]</p> <p>8. DATE: 08/14/13</p>	<p>FRENCH & PARRELLO</p> <p>REGISTERED PROFESSIONAL ENGINEERS</p> <p>1000 PLAINSBORO ROAD, SUITE 100, PLAINSBORO, NJ 08540</p> <p>TEL: 609.426.1000 FAX: 609.426.1001</p> <p><i>Andrew L. French, P.E.</i></p> <p>ANDREW L. FRENCH, P.E.</p> <p>PROFESSIONAL ENGINEER, N.J. REG. NO. 40884</p>	<p>SITE PLAN</p> <p>PRELIMINARY AND FINAL MAJOR SITE PLAN</p> <p>SKILLED NURSING FACILITY</p> <p>BLOCK 1703, LOT 201</p> <p>1.00 ACRES (APPROX.)</p> <p>100' X 100' X 100' X 100'</p> <p>DATE: 08/14/13</p> <p>SCALE: AS SHOWN</p>
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3.3.1 Permitted Uses in the SNF

A. Principal Uses, as follows:

1. Patient rooms and treatment areas, for both inpatient and outpatient treatment and care.
2. Support facilities, such as medical laboratories, medical clinics, diagnostic testing, physical therapy and pharmaceutical facilities.
3. Facility operations, including but not limited to food service, laundry facilities, housekeeping and maintenance and storage facilities for medical supplies and equipment.
4. Offices for facility administrators and employees.

B. Accessory Uses and Facilities, as follows:

1. Overnight accommodations and eating facilities for on-duty facility employees.
2. Cafeterias for visitors and employees.
3. Off-street parking, limited to surface parking only, and loading facilities.
4. Outdoor and indoor gardens, open spaces and landscaped areas.
5. Utility and heating, air conditioning and ventilating facilities.
6. Accessory retail facilities, such as barber shops and beauty parlors, and facilities for the sale of sundries such as newspapers, personal articles, gifts, flowers, food and similar convenience products for residents and their guests.
7. Other accessory uses customarily incidental to skilled nursing/rehab facilities.

3.3.2 Area, yard and building requirements for the SNF

Standard	Required/Allowed
Minimum lot area	5 acres
Minimum lot frontage	300 feet
Minimum building setbacks from perimeter lot lines: - Plainsboro Road - Other lot lines	50 feet 25 feet
Minimum setback of building to driveways and parking lots	10 feet
Minimum setback of parking areas from perimeter lot lines: - Plainsboro Road - Public Park - Other lot lines	25 feet 25 feet 15 feet
Maximum building height (feet/stories)	40 feet/2 stories
Maximum floor area ratio (FAR)	0.50
Maximum impervious coverage on lot	60%

3.4 The Public Park

The area shown on the amended Redevelopment Area Land Use Plan as the Public Park will limit future development to small-scale improvements associated with the Public Park. The public park,

at completion, is to be a publicly-accessible, passive open space facility. The park shall continue to be developed in the following manner: preservation of as much of the natural vegetation that exists on the property as possible; a single driveway access and off-street parking area of approximately 25 off-street spaces for visitors; improvements limited to a circular paved pathway within the park; other smaller supportive facilities such as bike racks, gazebos, sheds, shelters, restrooms, benches, viewing areas and non-paved walking paths; continued remediation, replanting and care for formerly developed areas with lawn, shrubs, trees and other self-sustaining indigenous plant materials. The overall vision of the Public Park is to create a natural meadow on the upland portion, and to preserve wetland and associated waterfront vegetation along the Millstone River. To the extent permitted, non-paved walkways shall be provided down to the Millstone River waterfront, and benches and other sitting areas shall be provided to afford visitors views of the water. Small recreational structures limited to those listed above and other similar structures shall be the sole improvements permitted within the remainder of the park.

The Public Park has been cleared of all existing buildings. The off-street parking area, circular pathway and bicycle rack have been constructed, and replanting of the cleared and disturbed area is also substantially complete. The circular pathway has been provided within the park linking it to the pedestrian and bicycle pathway system which connects it to all of the areas within the Redevelopment Area.

3.4.1 Permitted Uses and Structures in the Public Park:

1. Passive recreational facilities, including wetland mitigation areas, meadows and lawn areas, paved walking paths, and where permitted, low-impact, handicapped-accessible walking trails to the banks of the Millstone River, benches, gazebos or shelters.
2. Off-street vehicular and bicycle parking including bike racks.
3. Rest rooms and maintenance sheds.

3.4.2 Area, yard and building requirements for the Public Park

Standard	Required/Allowed
Minimum lot area	30 acres
Minimum lot frontage	2,000 feet
Minimum setback of building and parking areas from Plainsboro Road and Route 1	55 feet
Maximum building height	15 feet/1 story
Maximum impervious coverage	3%

3.5 The Pediatric Medical Office (PMO)

The Pediatric Medical Office district is intended to facilitate the development of a pediatric medical care facility. It is anticipated that the development of the single building in the PMO will occur in two or three phases, with a total buildout of 100,000 square feet of development. The PMO shall be in-

egrated into the overall Redevelopment Area through a swath of park-like open space, a minimum of 100 feet wide running along the east side of Punia Boulevard and the south side of Hospital Drive—referred to as the “green corridor”—as well as other linkages to the comprehensive pedestrian, bicycle and vehicular circulation system within the Redevelopment Area. Anything in this plan notwithstanding, in no event shall any buildings or parking areas or detention basins encroach into the Green Corridor. This also includes a direct vehicular connection to development within the ORC. The first phase of the PMO development shall provide an attractive landscaped area with pedestrian paths within the portion of the property planned for the second and third phases of development, where future buildings and parking areas are to be located, and shall be utilized in the interim for recreational purposes until that portion of the property is developed.

3.5.1 Permitted Uses for the PMO

A. Principal uses, as follows:

1. Patient rooms and treatment areas, for outpatient treatment and care.
2. Support facilities, such as medical laboratories, medical clinics, diagnostic testing, physical therapy and pharmaceutical facilities.
3. Facility operations, including but not limited to food service, laundry facilities, housekeeping and maintenance and storage facilities for medical supplies and equipment.
4. Offices for facility administrators and employees.

B. Accessory uses and facilities, as follows:

1. Outdoor and indoor gardens, open space and landscaped areas.
2. Off-street parking and loading facilities.
3. Other accessory uses, customarily incidental to the principal permitted uses in the PMO.

3.5.2 Area, yard and building requirements for the PMO

Standard	Required/Allowed
Minimum lot area	12 acres
Minimum lot frontage	500 feet
Minimum building setbacks from: - Centerline of Punia Boulevard - Property line along Plainsboro Road - Other lot lines	100 feet 100 feet 50 feet
Minimum setback of building to driveways and parking lots	10 feet
Minimum setback of parking areas from: - Centerline of Punia Boulevard - Property line along Plainsboro Road - Other lot lines	100 feet 100 feet 25 feet
Maximum building height (feet/stories)	60 feet/4 stories
Maximum floor area ratio (FAR)	0.4
Maximum impervious coverage on lot	60%

3.6 The Day Care Complex (DCC)

The Day Care Complex encourages the development of day care uses for children and/or adults, as well as education facilities for persons with special needs. Two buildings shall be permitted within the DCC. The DCC shall be connected to the rest of the Redevelopment Area via the comprehensive pedestrian, vehicular and bicycle circulation system.

3.6.1 Permitted Uses for the DCC

A. Principal uses, as follows:

1. Child care centers, subject to the minimum standards of the Plainsboro Zoning Ordinance § 101-65F. However, where standards conflict, the standards set forth in this amended Redevelopment Plan shall apply.
2. Special education for children with learning disabilities or psychological disorders.
3. Adult day care for adults with chronic health conditions or who otherwise need supervised care outside the home during the day.

B. Accessory uses and facilities, as follows:

1. Outdoor play areas for children, and play equipment storage sheds.
2. Outdoor gardens, open space and landscaped areas.
3. Off-street parking and drop-off/pickup facilities.
4. Other accessory uses customarily incidental to the principal permitted uses in the DCC.

3.6.2 Area, yard and building requirements for the DCC

Standard	Required/Allowed
Minimum lot area	4 acres
Minimum lot frontage	500 feet on Scudders Mill Road 500 feet on Hospital Drive 175 feet on Campus Road
Minimum distance between buildings	25 feet
Minimum building setbacks from: - Property line along Scudders Mill Road and Campus Road - Centerline of Hospital Drive - Other lot lines	50 feet 60 feet 25 feet
Minimum setback of building to driveways and parking lots	10 feet
Minimum setback of parking areas from: - Property line along Scudders Mill Road and Campus Road - Centerline of Hospital Drive - Other lot lines	25 feet 35 feet 15 feet
Maximum building height (feet/stories)	40 feet/2 stories
Maximum floor area ratio (FAR)	0.4
Maximum impervious coverage on lot	60%

3.7 The Adult Retirement Community (ARC)

The purpose of the Adult Retirement Community is to promote the development of an upscale adult retirement community in an attractive and well-planned environment that is integrated within the rest of the healthcare-oriented campus in the Redevelopment Area. All residential units within the ARC shall be restricted to adults 55 years of age or older, but may also be restricted to seniors 62 years of age or older, subject to state and federal laws applicable to such age-restricted developments. The buildings, grounds, parking and other accessory functions shall be designed and arranged as a comprehensive single, harmonious, architecturally compatible and integrated community, provided in an aesthetically pleasing environment, and at a pedestrian-friendly, human scale. The vehicular, pedestrian and bicycle circulation system for the ARC shall be integrated into that of the comprehensive circulation system for the Redevelopment Area, especially with regard to linkages to the Public Park and SNF components. Moreover, the design and construction of the ARC component shall be of an equally high standard as the remainder of the Redevelopment Area, and shall be similarly treated with respect to landscaping, signage, stormwater drainage and utilities, including the utilization of sustainable design and materials.

3.7.1 Permitted Uses in the ARC

A. Principal Uses, as follows:

1. Age-restricted residential units, or senior citizens housing, in the form of multi-story apartments.

B. Accessory uses and facilities, limited to the use and benefit of the residents, staff and guests of the Adult Retirement Community, which may include:

1. Indoor and outdoor recreation facilities, gardens and open space.
2. Libraries, club rooms and other common areas for residents.
3. Off-street parking facilities, which may include surface parking lots and below-grade or below-building enclosed parking garages, but not multi-level parking garages.
4. Other accessory uses customarily incidental to the principal permitted uses in the ARC.

3.7.2 Area, yard and building requirements for the ARC

Standard	Required/Allowed
Minimum lot area	35 acres
Minimum lot frontage	500 feet
Minimum building setbacks from perimeter lot lines: - Plainsboro Road - Other lot lines	75 feet 50 feet
Minimum setback of building to driveways and parking lots	10 feet
Minimum setback of parking areas from perimeter lot lines: - Plainsboro Road - Other lot lines	100 feet 50 feet
Minimum open space	50%
Maximum floor area ratio (FAR)	0.55
Maximum impervious coverage on lot	50%

3.7.3 Supplementary building requirements for the independent living units within the ARC

Standard	Required/Allowed
Maximum number of apartments per building	60 units
Maximum apartment building length	400 feet
Maximum apartment building height	85 feet/6 stories*
Minimum distances between buildings: -Side-to-side -All other (front-to-front, front-to-side, front-to-rear, rear-to-rear)	20 feet 40 feet

*Only for buildings with a “below-building” parking level. All other apartment buildings shall have a maximum height of 5 stories.

3.8 The Office and Residential Complex (ORC)

The Office and Residential Complex is intended for the development of general office uses, research laboratories and facilities, and a variety of continuing care residential uses, such as assisted living, dementia care, nursing and congregate care units. The ORC shall be designed and developed to be integrated with the other components of the amended Redevelopment Plan, including the comprehensive circulation system for the Redevelopment Area. In addition, a Bus Rapid Transit (BRT) stop (see further discussion in Section 4.1.3) and reflecting pool shall be provided along the Punia Boulevard frontage of the complex.

3.8.1 Permitted Uses for the ORC

A. Principal uses, as follows:

1. General administrative and professional offices.

2. Research laboratories engaged in scientific investigation, testing or the production of factual information for industrial, commercial or institutional clients or patrons where no tangible or physical product for general marketing is directly produced therein and where no danger, hazard or nuisance shall extend beyond the borders of any lot which exceeds that created by such similar activities as are permitted as education-research uses in the PMUD Zone.
3. Congregate Care units.
4. Assisted living units, consistent with the definition in Section 101-137O(3) of the Plainboro Zoning Regulations.
5. Dementia care.
6. Hospice care.

B. Accessory uses and facilities, as follows:

1. Assembly halls or meeting or conference rooms for meetings incidental to the business of the principal use.
2. Restaurants or cafeterias, primarily for supplying meals only to employees and guests of the principal use, and newsstands, post offices, branch banking facilities and similar conveniences serving primarily employees and residents of the principal use, provided that there shall be no external evidence of such use.
3. Indoor and outdoor recreation facilities, provided that all such accessory buildings and uses shall be planned as an integral part of the principal use development.
4. Custodial living quarters.
5. Off-street parking and loading facilities.
6. Transit facilities, including Bus Rapid Transit stops.
7. Other accessory uses customarily incidental to the principal permitted uses in the ORC.

3.8.2 Area, yard and building requirements for the ORC

It is anticipated that the ORC could be developed as a complex of 1 or more buildings. The ORC shall be designed and developed to be integrated with the other components of the Redevelopment Area. In all cases, the vehicular, pedestrian and bicycle circulation system shall be integrated into that of the comprehensive circulation system for the Redevelopment Area.

Standard	Required/Allowed
Minimum lot area	4 acres
Minimum lot frontage	250 feet
Minimum building setbacks: - Centerline of Punia Boulevard - Centerline of Hospital Drive - Other lot lines	100 feet 75 feet 50 feet
Minimum setback of building to driveways and parking lots	10 feet
Minimum setback of parking areas from: - Centerline of Punia Boulevard - Centerline of Hospital Drive - Other lot lines	100 feet 75 feet 15 feet
Maximum building height (feet/stories)	60 feet/4 stories
Maximum floor area ratio (FAR)	0.4
Maximum impervious coverage on lot ⁷	60%

3.9 Hospital Auxiliary District (HAD)

3.9.1 Permitted Uses in the HAD

A. Principal uses, as follows:

1. Overflow parking for uses permitted within the HMOC. However, no parking spaces in the Hospital Auxiliary District (HAD) shall be counted towards satisfying the requirements for parking for any development within the HMOC, including a hospital, medical office, fitness center or any other principal or accessory use permitted therein, nor any other development within the Redevelopment Area.
2. Helipad limited to hospital use only meeting the applicable requirements of the FAA.
3. Stormwater management facilities.

B. Accessory Uses and facilities, as follows:

1. Storage sheds, provided that no shed exceeds 1,000 square feet of floor area.

⁷ Such impervious coverage excludes surface water bodies whether natural or manmade.

3.9.2 Area, yard and building requirements for the HAD

Standard	Required/Allowed
Minimum lot area	10 acres
Minimum lot frontage	500 feet
Minimum building setback from: - Route 1 and Scudders Mill Road right-of-way - Other lot lines	50 feet 30 feet
Maximum building height	15 feet/1 story
Minimum setback of building to driveway and parking lot	10 feet
Minimum setback of parking areas and helipad: - From Route 1 and Scudders Mill Road right-of-way - Other lot lines	50 feet 30 feet
Minimum open space	70%
Maximum floor area ratio (FAR)	0.003
Maximum impervious coverage on lot ⁸	30%

⁸ The developer is encouraged to utilize porous paving for all driveway and parking areas to reduce stormwater runoff volumes and to reduce the size of surface stormwater detention basins.

4.0 REDEVELOPMENT AREA CIRCULATION AND OPEN SPACE PLAN

Streets, walkways, bikeways, parks and other common open spaces constitute the “public realm,” the primary spaces that provide a framework and identity for the character of the Redevelopment Area. Beyond their strict utilitarian function, they have the potential to also create an inviting environment for residents, employees, patients, and visitors to the Redevelopment Area. They also provide the physical framework within which development within the individual components of the Redevelopment Plan will take place. In *Figure 9, Redevelopment Area Circulation and Open Space Plan*, the framework of required streets, walkways, bikeways, transit stops, and open space is shown.

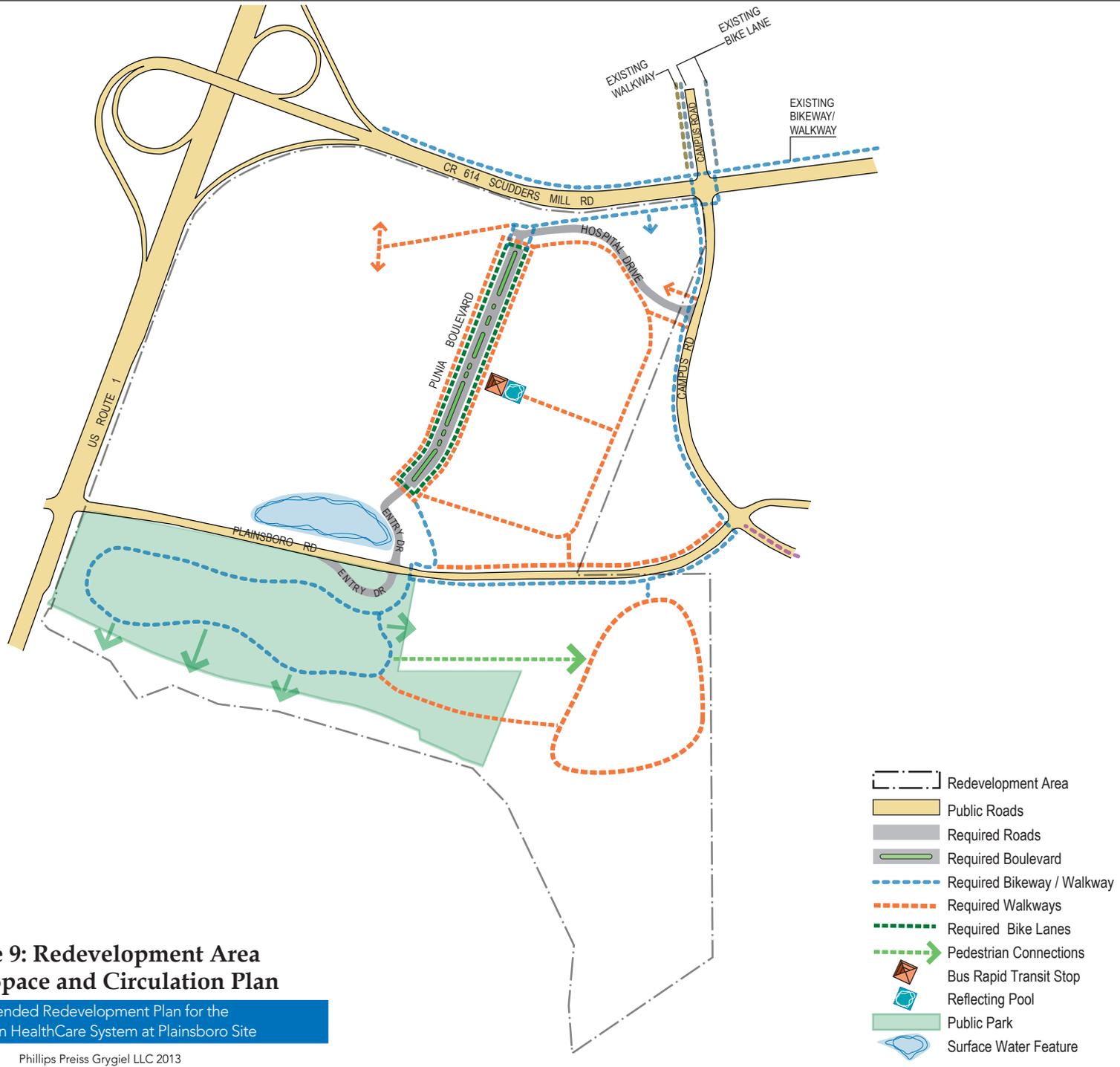
4.1 REDEVELOPMENT AREA CIRCULATION PLAN

The Redevelopment Area Circulation Plan is intended to accommodate multi-modal access and circulation within the Redevelopment Area, by accommodating a highly functional and logically laid-out system of automobile, bus, truck, bicycle and pedestrian circulation.

Punia Boulevard is the Redevelopment Area's “spine”, starting as an Entry Drive at a jughandle entrance on Plainsboro Road and proceeding northward to connect with Hospital Drive at the northeast corner of the Redevelopment Area (see **Figure 9**). Hospital Drive is a required roadway linking Punia Boulevard to Campus Road to the east. Other entrances, roadways and driveways serving other components and land uses within the Redevelopment Area shall be at the discretion of the developer, but subject to review at the time of site plan approval.

The Redevelopment Area's bikeway and walkway circulation elements that connect to the existing and planned bikeway and walkway system elsewhere in Plainsboro are also fixed. The alignment of bikeway and walkways adjacent to Punia Boulevard is also fixed (see **Figure 9**). Other bikeways and walkways must be provided to facilitate access and circulation within the Redevelopment Area. However, their location and alignment are flexible, although also subject to site plan review and approval.

The overall Redevelopment Area circulation plan is intended to balance the mobility needs of vehicular traffic with the safety and comfort of pedestrians and cyclists through mandated walkways and bikeways as well as traffic-calming measures such as crosswalks and bumpouts. While the only streets in the Redevelopment Area with fixed alignments are Punia Boulevard and Hospital Drive, guidelines for additional driveways and roadways with flexible alignments are included in this Redevelopment Plan.



**Figure 9: Redevelopment Area
Open Space and Circulation Plan**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

4.1.1 Required Primary Streets

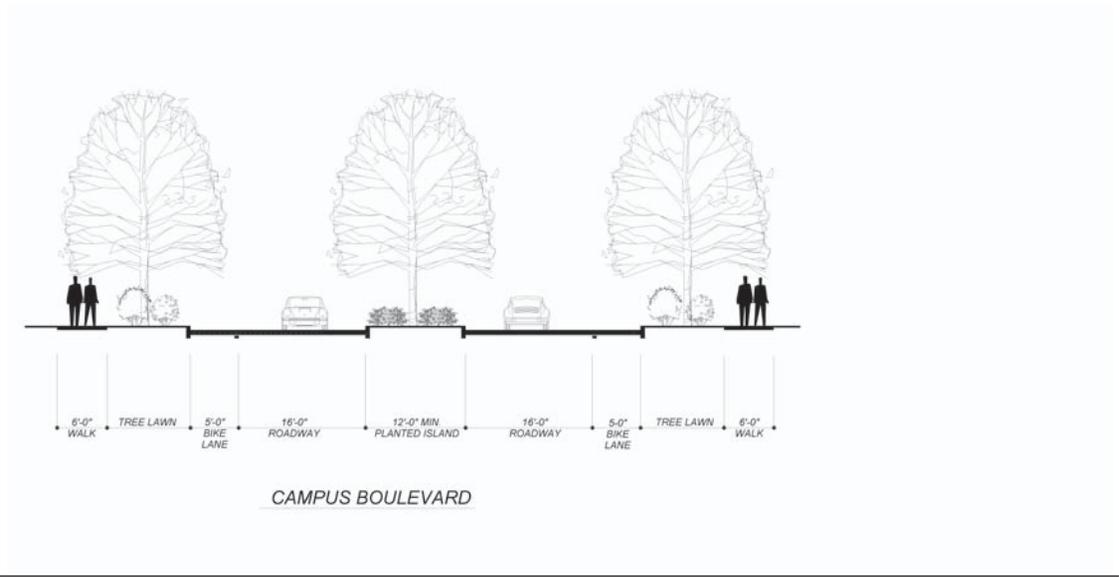
Punia Boulevard is the only required primary street within the Redevelopment Area. Its alignment shall match exactly (or very closely) that shown in **Figure 9**. Punia Boulevard has four parts, as follows:

- (1) It starts as a jughandle intersection on Plainsboro Road and provides access into and out of the amended Redevelopment Area where the HMOC and the PMO components meet.
- (2) The street then curves in a northwesterly direction within the Redevelopment Area. This portion of Punia Boulevard comprises two lanes in the inbound or northerly direction, and two lanes in the outbound or southerly direction. One lane of the outbound roadway is designated as left-turn only, the other as right-turn only. A landscaped median of minimum eight (8) foot width, planted with trees, is provided between the two traffic directions.
- (3) Where Punia Boulevard turns right and separates itself from the entryway into the PMO parking lot, as shown in Figure 9, its cross-section, as shown in Figure 10, **Required Street Sections**, has one vehicular travel lane and one five-foot bicycle lane in each direction, separated by a planted median of a minimum of 12 feet in width. The median must also provide for left-turn stacking lanes for driveways which connect westward into the HMOC and eastward into the PMO, as necessary. The bike lanes shall be marked and signed as “bike lane.” The specifications shown in the “Punia Boulevard” street section in **Figure 10** shall be followed.
- (4) At the northerly end of the Redevelopment Area, “Punia Boulevard” intersects with “Hospital Drive,” which in turn connects to Campus Road in the northeast corner of the Redevelopment Area.
- (5) The configuration of lanes, sidewalks, trees and bike lanes along Punia Boulevard shall be exactly as, or close to, the section shown in **Figure 10**, although a narrower tree median is permitted if necessary in that short, curving section of Punia Boulevard which provides access into the Redevelopment Area from Plainsboro Road.

Hospital Drive

“Hospital Drive” is required to connect Punia Boulevard to Campus Drive to the east, as shown in **Figure 9**.

10a Campus Boulevard



10b Access Driveways and Roads

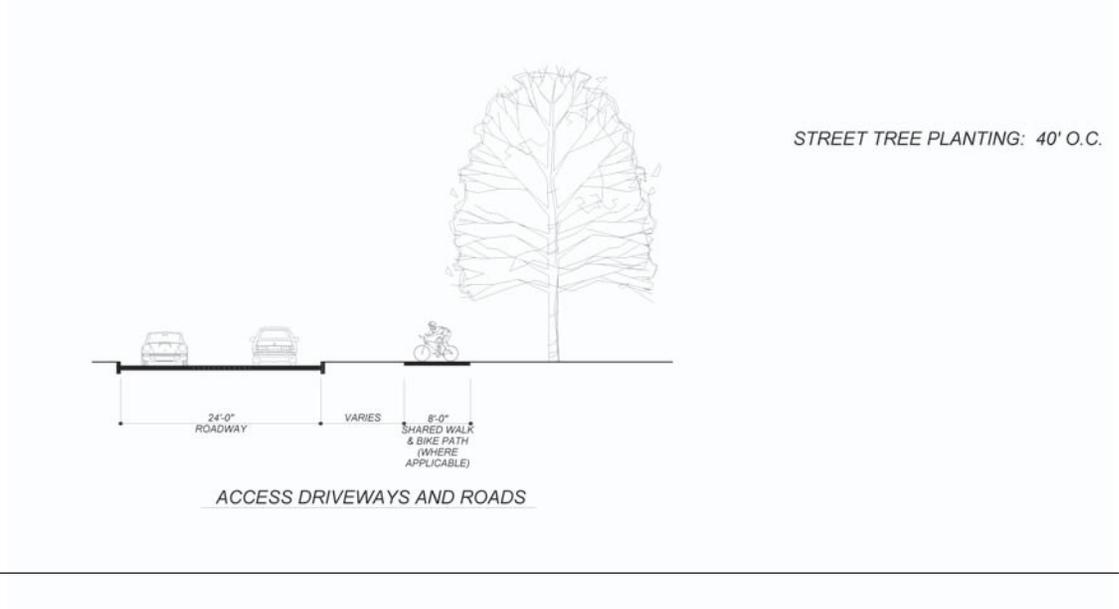


Figure 10: Required Street Sections within the Redevelopment Area

Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013

Source:

4.1.2 Secondary Roadways and Driveways

Within the Redevelopment Area, secondary roadways and driveways are necessary for access to each component of the amended Redevelopment Plan—the HMOC, SNF, PP, PMO, DCC, ARC, ORC and HAD. The widths and configurations of such driveways, and the extent to which sidewalks and shade tree plantings are to be provided in conjunction with them, will depend upon their function, and in particular, their expected level of traffic and types of vehicles which utilize them. Driveway intersections shall be located with consideration given to the adequacy of sightlines and distances.

Such specifications shall be guided by the standards for driveways in the Plainsboro Subdivision and Site Plan Review regulations (which are specifically incorporated by reference herein for this purpose). However, for areas of the Redevelopment Area which are developed to be purely residential in nature, driveways shall meet standards of the New Jersey Residential Site Improvement Standards.

Overall, the secondary driveway network must provide short and convenient routes to and from destinations within the Redevelopment Area, help distribute vehicular traffic so that no one driveway or street becomes overloaded, and provide the foundation upon which a pedestrian- and bike-friendly environment can be built. Adjacent parking areas shall be interconnected to provide alternative means of ingress and egress to each site.

4.1.3 Transit

The HMOC and the SNF are currently served by transit services from NJ Transit, the County of Middlesex, Monroe Township and Princeton University. The 600 and 655 bus service provided by NJ Transit makes regularly scheduled stops at the existing transit stop on Punia Boulevard for the SNF. The stop along the westerly side of Punia Boulevard will have a companion stop along the easterly side when adjacent properties develop. The Bus Rapid Transit will be discussed in more detail in the next section.

Bus No. 600 is a regional route providing service in a north-south direction for Plainsboro residents and surrounding communities. In addition to the hospital campus, the service also makes regular stops at the Princeton Junction train station which serves NJ Transit's northeast corridor.

Bus No. 655, which began service in May 2012, provides much-needed east-west connectivity between the economic centers of Plainsboro Township and Princeton Borough. This one-of-a-kind service was made possible through the cooperation of many funding partners in addition to NJ Transit, which include the hospital, Middlesex County, Princeton University, the North Jersey Transportation Planning Authority, the Delaware Valley Regional Planning Commission, and the Greater Mercer Transportation Management Agency.

In addition to NJ Transit buses, the County of Middlesex operates the Plainsboro-Cranbury-Jamesburg M-6 shuttle providing residents in these communities with an opportunity to access the hospital and adjacent facilities. Frequency of service is currently operating at approximately one hour intervals; additional service with more frequent service may be instituted when additional funding sources are made available.

Schedules for the three services mentioned above are available on Plainsboro Township's website (www.plainsboronj.com) or on paper copy at the Municipal Building, Library and other locations throughout the Township.

4.1.4 Bus Rapid Transit (BRT) Stop

The Bus Rapid Transit (BRT) program, which is under the auspices of NJ Transit, is intended to enhance the attractiveness of using buses as an alternative to automobile travel, particularly the use of single-occupancy vehicles for commuting purposes. The BRT is intended to reduce commuting times through the provision of fewer local stops, dedicated lanes on highways, and increased frequency of bus travel, ultimately providing a much higher quality of service than traditional bus service. Increased BRT use will reduce traffic congestion, which in turn also mitigates pollution and losses in productivity.

The Bus Rapid Transit stop shall be improved and made permanent on the east side of Punia Boulevard at the southwest corner of the ORC development. This location is convenient and safe for pedestrian access from the HMOC, ORC, DCC and PMO and is across the street from the SNF and the ARC. The necessary road configuration shall be provided to allow the bus to pick up and drop off passengers without blocking or interfering with other vehicles or cyclists on Punia Boulevard. A passenger waiting shelter shall be provided, with seating, lighting, signage, and a BRT system-wide map.

4.1.5 Bicycle and Pedestrian Connections within and outside of the Redevelopment Area

A bicycle and pedestrian pathway system shall be developed with the following three elements:

- (1) connections to the existing and planned bicycle and pedestrian pathway system within Plainsboro outside of the Redevelopment Area;
- (2) provisions for convenient and safe walking and cycling within each component or land use within the Redevelopment Area, as appropriate; and
- (3) provisions for pedestrian and bicycle linkages between these components or land uses within the Redevelopment Area, with particular emphasis on providing access from all components to the Public Park.

Walkways and bikeways shall be provided as shown in **Figure 12**, and as described below.



**Figure 11: Bus Shelter on Plainsboro Road
in front of Merwick Care Center**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: PPG 2012

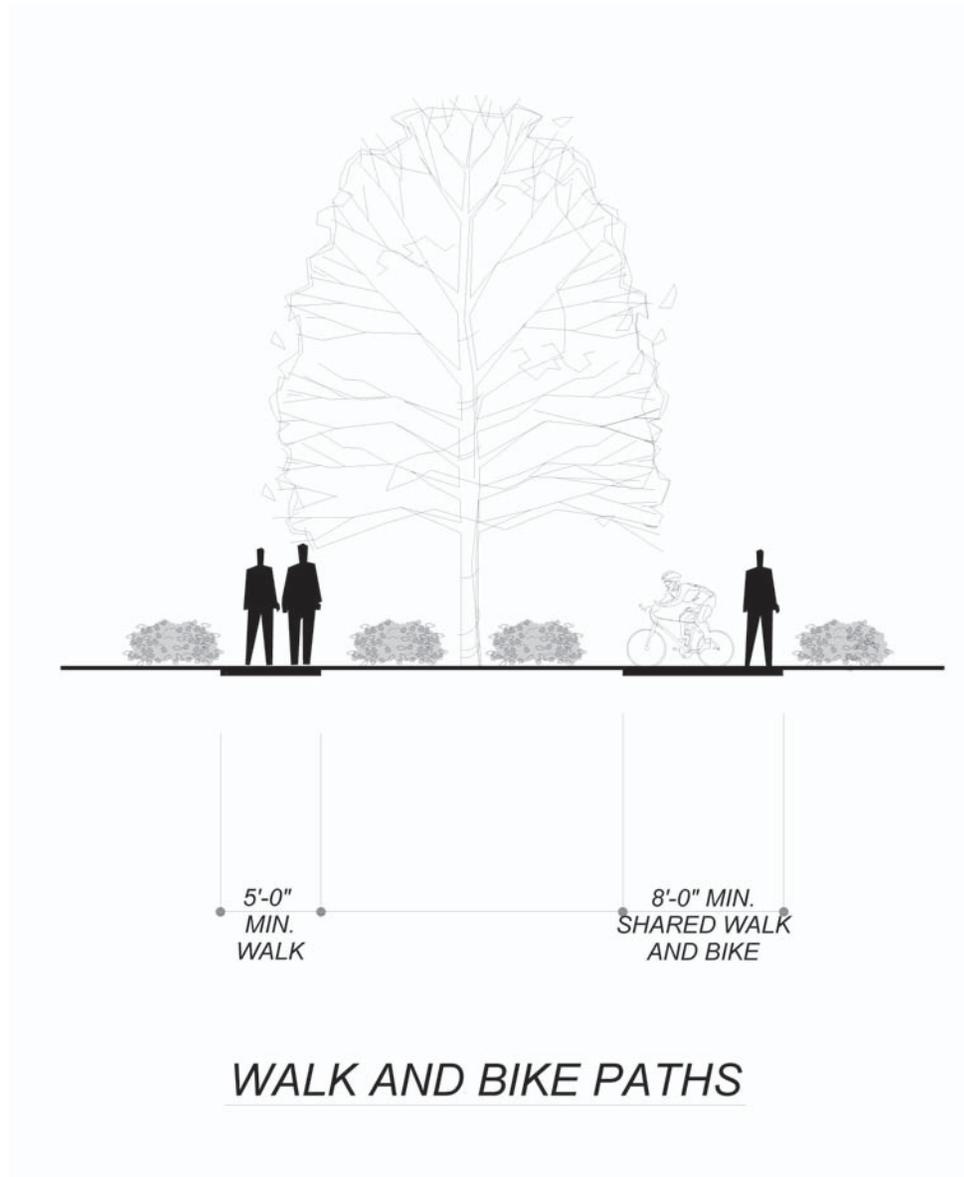


Figure 12: Walkway and Bikeway Sections within the Redevelopment Area

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013

Source:

- (a) A six (6) foot wide walkway shall be provided on the north side of Plainsboro Road from the entry portion of Punia Boulevard to Campus Road. An eight (8) foot wide bike path/walkway shall be provided on the south side of Plainsboro Road from the entry portion of Punia Boulevard to Campus Road. At the intersection of Punia Boulevard and Plainsboro Road, a signalized crosswalk shall be provided across Plainsboro Road. The bike path/walkway shall continue southward from this intersection into the Public Park, as described in (e) below.
- (b) The bike path/walkway shall continue into the Redevelopment Area, from the north side of Plainsboro Road along the eastern side of Punia Boulevard. The bike path/walkway shall run along the west side of the berm to be provided just to the northeast of the intersection of Punia Boulevard and Plainsboro Road. A walkway only shall continue north through the Green Corridor.
- (c) The bike path/walkway shall also run along the west side of the berm, but at the first driveway entrance into the PMO, it shall run along both sides of the wider portion of Punia Boulevard (where the central median is at least 12 feet wide), in-street via the required bike lanes, which were described earlier in **Section 4.1.1, Required Primary Streets**. A walkway or sidewalk shall run along both sides of Punia Boulevard. On the east side of Punia Boulevard, this pedestrian walkway may be directly adjacent to the street, or may meander through the Green Corridor (see Section 4.2.2).
- (d) At the northern terminus of Punia Boulevard, the in-street bike paths shall cross Hospital Drive via a crosswalk. The bike path/walkway shall continue eastward along the south side of Scudders Mill Road to the intersection of Scudders Mill Road and Campus Road. At the intersection of Scudders Mill Road and Campus Road it shall connect to the Township's bike path system outside of the Redevelopment Area. (See 2008 Township of Plainsboro Bikeway Plan and Pedestrian Circulation Plan.) A pedestrian walkway shall also continue westward along the north side of the driveway extension of Hospital Drive, connecting it to the parking areas of the HMOC and HAD.
- (e) A single, meandering, looped, handicapped-accessible bike path/walkway shall be provided within the Public Park, with a configuration similar to that shown in **Figure 9**. Such an alignment is not fixed, but shall be planned and refined based upon topographic, soil, vegetative and other environmental conditions in the Public Park, and with consideration given to the remediation of areas which were formerly utilized for buildings, driveways, parking, and other improvements of the FMC operation.
- (f) Additional walkway connections which provide access to sitting areas along the Millstone River shall be provided as permissible under the DEP and DRCC regulations. Walking trails through vegetated areas, such as along the banks of the Millstone River, shall not be paved. The redeveloper and the Township shall together seek waivers where appropriate, to allow for such improvements to be placed within these areas.

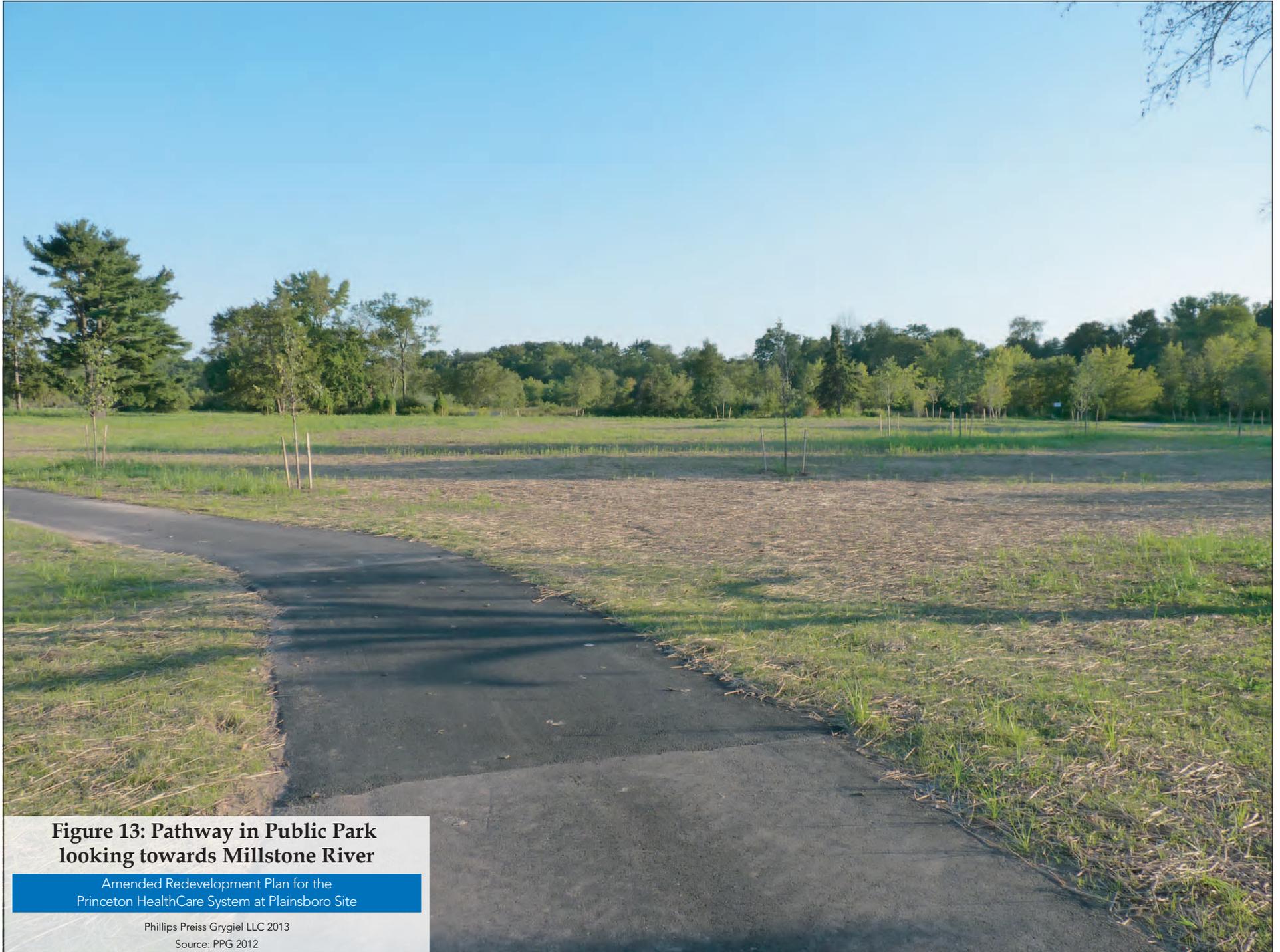
- (g) A walkway extension from the looped bike path/walkway within the Public Park shall be provided, connecting to both the ARC and SNF. Additionally, a looped, handicapped-accessible, pedestrian path shall be provided around the entire perimeter of the ARC. This path shall link to, and may coincide with, the required pathways linked to the Public Park. The pathway sections within the SNF and ARC shall, to the extent permitted by the DEP and DRCC, be paved with smooth materials conducive to wheelchairs and shall connect to at least one entrance of each of the principal buildings of the SNF and ARC.
- (h) A pedestrian-only walkway shall be provided which extends the walkway within the Green Corridor around the eastern perimeter of the ORC and PMO components, and the southern perimeter of the PMO. This walkway shall also be connected to the walkway which runs from Campus Road and Punia Boulevard on the north side of Plainsboro Road at the southeast corner of the PMO, and also to the bike path/walkway which runs along the west side of Campus Drive (outside of the Redevelopment Area) at the northeast corner of the ORC. Additionally, a pedestrian pathway shall be provided along the southern border of the ORC component, connecting Punia Boulevard to the outer perimeter pedestrian pathway. This pathway shall also connect to the BRT stop, as depicted in **Figure 9**.
- (i) Walkways shall also connect the primary building entrances within the DCC to one another, as well as to the walkways along Hospital Drive and the bike path/walkway which runs along the west side of Campus Drive outside the Redevelopment Area.
- (j) A bike path/walkway connection shall be provided from the DCC's internal pedestrian and vehicular parking area to the bike path/walkway which runs along the south side of Scudders Mill Road between Punia Boulevard and Campus Road.

Additional bike paths, sidewalks and walkways are encouraged to be provided in all of the other components of the Redevelopment Area, except for the Public Park, where only those depicted in **Figure 9** may be permitted. Bumpouts, striped crosswalks, signage and signals, as necessary, shall be provided as appropriate for safety, convenience, and traffic calming.

Design of Bikeways and Walkways

Walkways shall be at least six (6) feet wide. Bike path and combined bike path/walkways shall be at least eight (8) feet wide. In-street bike lanes shall be at least five (5) feet wide.

Crosswalks shall be provided across Punia Boulevard at all points where the primary vehicular entry to each component (HMOC, PMO, ORC) of the Redevelopment Plan connects to Punia Boulevard, as well as at the required BRT stops. Crosswalks shall also be located with consideration given to the adequacy of sightlines and distances.



**Figure 13: Pathway in Public Park
looking towards Millstone River**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: PPG 2012



Figure 14: Vehicular Parking Lot and Pathway in Public Park

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: PPG 2012

4.2 REDEVELOPMENT AREA OPEN SPACE PLAN

The Public Park, the ±30-acre portion of the Redevelopment Area located south of Plainsboro Road along the Millstone River, is the signature open space portion of the amended Redevelopment Plan, providing a visual and passive recreational amenity to the immediate Plan Area as well as to all residents and visitors from the Plainsboro community and beyond.

The location, configuration, access, design, and integration of other open spaces within each component of the Redevelopment Area is equally important.

The exact location, function, and improvement of common open spaces within each component of the Redevelopment Area is flexible, but such open spaces shall be consciously-designed elements, rather than vestigial spaces, and a vital part of the redeveloped environment.

4.2.1 Public Park

The Public Park is a required element in the amended Redevelopment Plan, and fixed in terms of its location and extent, as shown in **Figure 9**. The primary purpose of the park is passive recreation. All existing buildings, driveways, parking and other fixed improvements that existed within this portion of the former FMC campus shall be removed, with the exception of a single driveway off Plainsboro Road, leading to off-street parking for 25 to 30 cars. The Park grounds shall be remediated and restored to a natural state, with preservation of vegetation along the Millstone River waterfront, and the creation of a “meadow” environment on the remainder. The “meadow” shall have rolling or level topography, with natural-height, unmown native meadow grasses and herbaceous plants interspersed with “lawn” areas of mown native vegetation and clusters of trees. Efforts shall be made to retain as much of the existing vegetation and topography as possible.

The park shall also provide a low-impact, paved, looped walking trail, as discussed earlier in **Section 4.1.4(g)**. Bicycle racks shall be provided where the bike path intersects with the walking trail a short distance into the park.

Accessory structures supportive of the park’s passive recreational focus, such as benches, a gazebo, rest rooms, a maintenance or storage shed, and others customarily incidental to passive recreation activities, are also permitted and encouraged.

The Public Park shall be publicly accessible, and open to residents and visitors both within and outside of the Redevelopment Area, and from within and outside of Plainsboro.

4.2.2 Private and Common Open Spaces

Private Open Spaces within each Component. Each component of the Redevelopment Area, outside of the Public Park, is expected to provide private open space for the use and enjoyment of the residents, employees and visitors within said component. Such open space shall be of a size

and configuration appropriate to the principal use, and the activities or intended use of each of the open spaces shall be appropriate to the function of each of the components. Thus, for example:

- ⤴ The HMOC shall provide healing gardens and educational gardens as well as open spaces to provide visual relief to the areas of impervious surfaces devoted to parking, driveways and buildings.
- ⤴ The PMO shall provide open space for physical therapy and outdoor exercise for the patients of the facility.
- ⤴ The SNF shall, at a minimum, provide linkages and access to the Public Park for similar purposes, which may be supplemented by on-site open spaces. Depending upon the nature of the uses which are developed, the ORC shall provide open space for residents, throughout the day and also to allow employees and visitors to sit outside during lunch-time. They are also encouraged to provide outdoor open space to serve the active recreational needs of residents and employees where appropriate, and walking trails within the properties, linked to the Public Park.

Common Open Space: the Green Corridor

A major common, or shared, open space in the form of a wide “Green Corridor” is required along the east side of Punia Boulevard from Plainsboro Road to Hospital Drive, and extending eastwards along the south side of Hospital Drive to Campus Road. The minimum width of the Green Corridor shall be 100 feet adjacent to Punia Boulevard and 75 feet adjacent to the building and parking areas fronting Hospital Drive. Between Hospital Drive and stormwater retention basins, the minimum width of the Green Corridor may be reduced to 35 feet, but shall be landscaped with a solid screen of evergreen plantings to a height of no less than eight (8) feet.

Other than for the portion of the Green Corridor between Hospital Drive and a detention basin, the Green Corridor shall have a common landscape treatment along its entire length, through all the different components of the Redevelopment Area. Formal lawns, lightly interspersed with low plantings and clusters of shrubs and shade, ornamental and evergreen trees in a naturalistic manner, are required, as well as a variety of seating areas in shade and sun. Continuous plantings of perennial and ornamental trees shall be provided along all walkways and adjacent to vehicular entranceways through the linear park. Plantings along the property side of the linear park shall also function as screen buffers for parking areas, refuse enclosures and other site features that detract from the visual quality of the corridor. The overall appearance of the Green Corridor shall be open, spacious, and verdant. No stormwater detention or retention basins are permitted within the Green Corridor; however, the landscaping shall be designed to maximize infiltration.

The required walkways on the east side of Punia Boulevard – see Section 4.1.4(c) – are encouraged to be incorporated into the Green Corridor as a central, unifying feature. However, bike lanes are not permitted within the Green Corridor. Walking paths in the Green Corridor shall link the entrances of the PMO and ORC buildings to sidewalks or pathways along Punia Drive and Hospital Drive (where a building borders it).

No buildings, parking areas or detention basins shall be permitted to encroach into the Green Corridor, any minimum setback requirements in this Plan to the contrary notwithstanding.

4.2.4 Water Feature and Berms

Within the HMOC component of the amended Redevelopment Plan, north of Plainsboro Road and west of “Entry Drive,” the redeveloper shall provide a surface water feature—a retention basin for stormwater drainage—as a visual recreational amenity, as depicted in **Figure 15**. Fountains, planted islands and other natural features (as opposed to formal design with hard structures) are encouraged within and around this water feature.

A surface water feature designed as a reflecting pool shall be provided approximately at the midway point of the Green Corridor’s frontage on Punia Boulevard, and may be partially or fully located within the Green Corridor itself. The reflecting pool shall be designed as an attractive amenity that holds water at all times, and if possible should also detain stormwater.

On the east side of Punia Boulevard, directly adjacent to the Green Corridor and north of Plainsboro Road within the PMO, heavily landscaped berms shall be provided to buffer and screen the parking lot in front of the PMO buildings; however, such plantings are intended to screen the view of parked cars only, and views of the building itself shall be provided from Plainsboro Road.



**Figure 15: Retention Pond
in front of the Hospital**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013

Source: PPG 2012

5.0 REDEVELOPMENT AREA DESIGN GUIDELINES AND STANDARDS

The conceptual site plan for the Overall Redevelopment Area is shown in Figure 16. It provides the basis for the Redevelopment Area Design Guidelines and Standards set forth in greater detail in this chapter. The following guidelines and standards are specific to this amended Redevelopment Plan. Where applicable, reference is also made to relevant sections of the Township of Plainsboro's *Zoning* as well as *Subdivision and Site Plan Review* regulations.

For general standards relating to Planned Developments, refer to Sections §85-56.1, Provisions Stated; §85-56.2, Variations from Ordinary Standards; and §85-57, Planned Developments, in the Township's *Subdivision and Site Plan Review* regulations.

This Amended Redevelopment Plan recognizes that the uniqueness of each proposal may require flexibility and deviations from the standards and requirements set forth in this Chapter 5.0. The Planning Board may therefore, in its discretion, permit modifications or deviations from strict adherence to the requirements otherwise applicable to a particular facility where the Planning Board finds that strict adherence is not necessary in the interests of the public and that the waiver or modification is consistent with the intent and purpose of the underlying requirement and interests of the Township.

5.1 BUILDING STANDARDS

The intent of the building standards is to ensure a high-quality project that fits well into the landscape and surroundings. Specifically, buildings shall be designed to take advantage of natural and unique features of the site, and utilize design approaches which harvest natural light, minimize solar heat gain in summertime and heating loads in the winter, and follow other measures to reduce energy use and promote sustainability in design. Building orientation, vehicular circulation, and parking areas are also conceived to avoid any "back door" appearance within the entire Redevelopment Area.

5.1.1 Hospital/Medical Office Complex (HMOC)

The approved site plan of the planned hospital/medical office complex is shown in Figure 7. In Figures 17 through 20, photos of the recently completed phase 1 of the HMOC are provided for reference purposes.

The HMOC, when fully completed, will be composed of four primary buildings: the new construction of the Inpatient Bed Tower; the Diagnostic and Treatment Platform; the Medical Office Building; and the renovation of Building Two from the former FMC occupant of the Redevelopment Area.

A. Orientation

Primary orientation of the HMOC will be east-west, allowing the design to utilize natural day-lighting and control solar loads. The east-west orientation responds to the natural contours of the site and the orientation of the existing structure to be reused, and maximizes the visual and physical connections to the Millstone River and Public Park.

The southernmost building within the HMOC shall be the Inpatient Bed Tower. The Tower shall be the signature building of the hospital complex, with a concave façade facing Plainsboro Road and the Millstone River beyond.



AMENDED REDEVELOPMENT PLAN FOR THE PRINCETON HEALTHCARE SYSTEM AT PLAINSBORO SITE
 PLAINSBORO, NJ
 JANUARY 2013
 SCALE: NTS

J. ROBERT HILLIER
 LAND USE STRATEGY EXECUTION

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Figure 16: Conceptual Site Plan for Overall Redevelopment Area

Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
 Source: J. Robert Hillier



**Figure 17: View of Hospital Bed Tower
from Plainsboro Road**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013

Source: PPG 2012



**Figure 18: View of Hospital Bed Tower
from Punia Boulevard**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013

Source: PPG 2012



**Figure 19: View of Hospital Bed Tower
and Medical Office Building**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: PPG 2012



Figure 20: View of Medical Office Building from West

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: PPG 2012

A direct entrance to the Hospital Inpatient Bed Tower shall be provided within the curved southern façade. The southern entrance will provide access to a public concourse that links the primary areas of the hospital. The placement of the entrance shall align with an aisle of the parking lot to the south.

Entrances to the HMOC from the north shall be provided through a north-south extension of the public concourse as well as an entrance at the northern face of Building Two. Emergency department entrances, including ambulance drop-off and walk-in, shall be at the northern side of the Diagnostics and Treatment Platform.

Building Two shall resemble an “L” shape that partially encloses a landscaped area at the southern façade. The landscaped area shall include a healing garden and an outdoor area accessible to the education center. The primary public entrance shall be in the south facade, while the primary staff entrance shall be in the north façade. A fitness center shall be provided at the eastern end of this “L” shape, and shall open onto a pedestrian walkway that links directly to the Green Corridor along the east side of Punia Boulevard.

B. Massing

The overall massing of the hospital complex shall be broken into a series of buildings, each generally radiating off a public circulation concourse. Building heights shall exhibit a range of heights in order to create variety: low (2 to 3 stories), mid-rise (5 to 6 stories) and high-rise (8 to 10 stories).

The primary building in the HMOC, the Hospital Inpatient Bed Tower, shall have curved north and south façades, and the length of the curved façade shall be approximately 650 feet. The curve of the north façade shall follow the curvature of the south façade.

C. Horizontal Articulation

For all buildings in the hospital complex except for the Hospital Inpatient Bed Tower, building facades shall be designed and articulated in a way to avoid large uninterrupted masses and shall be designed to be sensitive to the human scale and the features of the site and the surrounding community. The massing of the various structures shall be broken down with carefully proportioned changes of plane in the vertical surfaces. Various design approaches shall be utilized to accomplish this, such as breaking long facades into a series of bays differentiated by projections or recesses.

In a similar manner, the horizontal massing of the south and north façades of the Hospital Inpatient Bed Tower shall be designed and articulated in a way to minimize their mass and present a more human scale, especially at the lower levels where the public will access the facility. The facades shall be broken up with a combination of changes in materials, textures and the utilization of projections and recesses that correspond to special public spaces on the patient bed floors. The result of the design shall be to create visual variety in the façade pattern.

In addition, the horizontal massing of the southern façade of the Hospital Inpatient Bed Tower shall be further broken up by design devices such as “solar screens” that control and make use of natural sunlight as part of the approach to sustainable design.

D. Materials

The facades of all buildings in the Hospital area shall be a palette of complementary materials, which shall include glass curtain walls, masonry and metal panels. Vertical elements shall divide each floor’s windows visually into a series of vertically-proportioned elements.

E. Building Loading

The loading dock for the HMOC shall be located within the western elevation, situated below grade. The siting of the loading dock shall take advantage of the natural topography and mature landscaping along Route 1 to substantially screen it from public view.

F. Bicycle Facilities

The hospital complex shall provide showers and changing rooms to allow staff to commute by bicycle. Bicycle racks shall be provided near north and south entrances to the building under a protected canopy, or within a secure locked room within the building.

G. Structured Parking

With respect to all future development in the HMOC, structured parking shall be required in accordance with the following schedule, and subject to the design standards set forth in Section 5.2.2 of this Plan:

1. Medical Arts Pavilion: Structured parking shall be required to accommodate the parking needs of any expansion to the Medical Arts Pavilion.
2. Additional floors on existing hospital building: Structured parking shall be required to accommodate the parking needs of any additional floors on the hospital building.
3. Incidental expansions:
 - a. Except as specifically set forth in 1 and 2 above, to address incidental expansions, a total of 10,000 additional square feet of development may be added in the HMOC without automatically triggering the need for structured parking. Each request for expansion shall be accompanied by a parking analysis demonstrating how much additional parking the proposed expansion will require. For purposes of this analysis, and any other parking ratios set forth in this Plan notwithstanding, the presumptive ratio shall be 5 spaces per 1,000 square feet of development. If the parking analysis shows that the expansion will

result in a site-wide deficiency, the Planning Board in its discretion may either require that structured parking be provided, or approve an alternate parking solution, to be used only until a parking structure is built.

- b. Structured parking shall be required to accommodate the parking needs of any additional development in the HMOC beyond the 10,000 square feet referenced in 3.a above.

5.1.2 Skilled Nursing Facility (SNF)

The approved site plan of the SNF is shown in **Figure 2**. **Figures 21 through 26** provide photographs of the completed Skilled Nursing Facility complex, known as the Merwick Care Center. Added staff parking is to be located at the complex's southern end on a portion of what was originally part of the Public Park, but is still owned by PHCS. It will be subdivided off from the PP and added to the SNF and utilized for added parking spaces in order to alleviate a shortage of parking at the complex. Amended subdivision and site plan approval shall be required. The subdivision will not have a material impact on the functionality of the PP and all required minimum tract area and width requirements both for the PP and SNF will still be met.

A. Orientation

The central portion of the SNF shall have an articulated façade facing Plainsboro Road; this is the primary, front facade. On either side of the central portion, connected wings shall be provided to house the Subacute Care and Long Term Care facilities.

Two public pedestrian entries shall be located within the façade of the central portion, facing Plainsboro Road. The service entrance shall be located at the rear, facing the Millstone River. The public entrances shall be highlighted within the articulated façade by means of voids or projections.

B. Massing

When viewed from the north and south, the SNF building complex should have buildings of varying heights, with a two-story height for the two wings. The façade plane of all facades of the SNF shall be broken into a series of bays each not to exceed 100 feet in length, and with most not to exceed 70 feet in length.

C. Materials

The materials of the SNF shall complement the HMOC and shall include glass curtain wall entrances, masonry and metal panels. The materials shall reflect the HMOC in such a way that the buildings appear to be part of the same health care campus. Panels for ventilation shall be located underneath each window and shall be used as a graphic element within the façade.

[Figure 21: Front Entry/Drop-off of Merwick Care Center]



**Figure 21: Front Entry/Drop-off
of Merwick Care Center**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013

Source: PPG 2012



**Figure 22: Courtyard within
Merwick Care Center**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: PPG 2012



**Figure 23: Courtyard within
Merwick Care Center**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: PPG 2012



**Figure 24: View of Merwick Care Center
from Public Park**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: PPG 2012



B1 ELEVATION- SOUTH PHASE 2B
18' x 10'



D1 ELEVATION- SOUTH PHASE 2B (CONT.)
18' x 10'



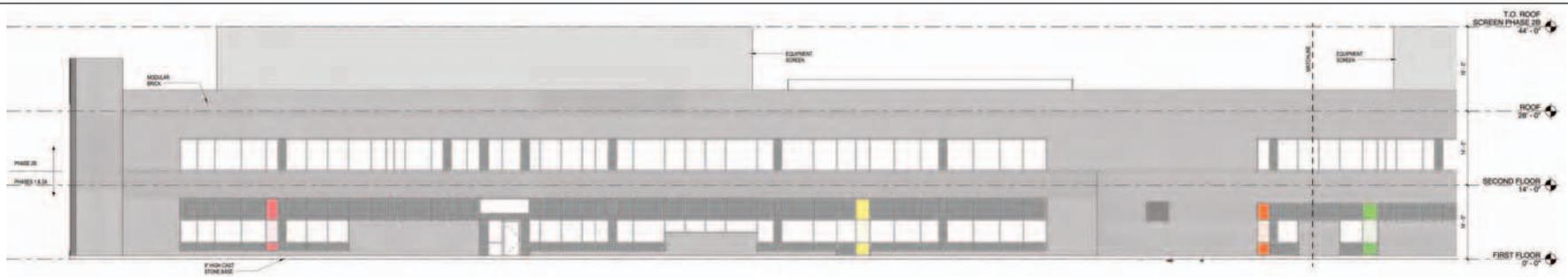
F1 ELEVATION- WEST PHASE 2B
18' x 10'

Figure 25: South and West Elevations of the Pediatric Medical Office

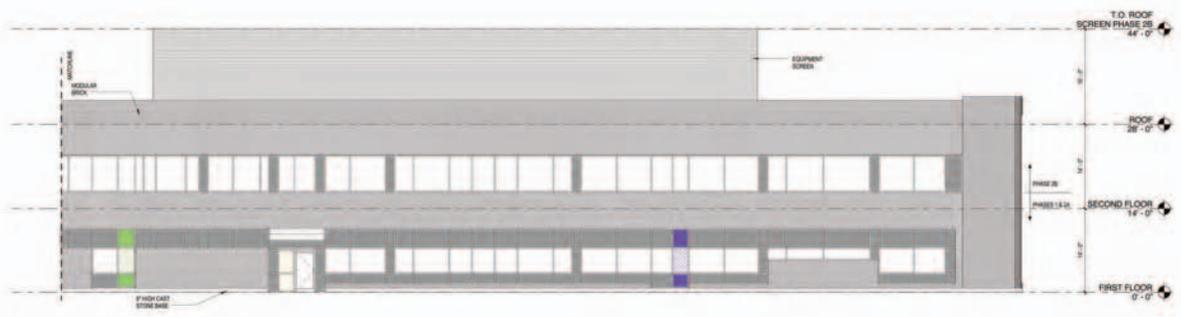
Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013

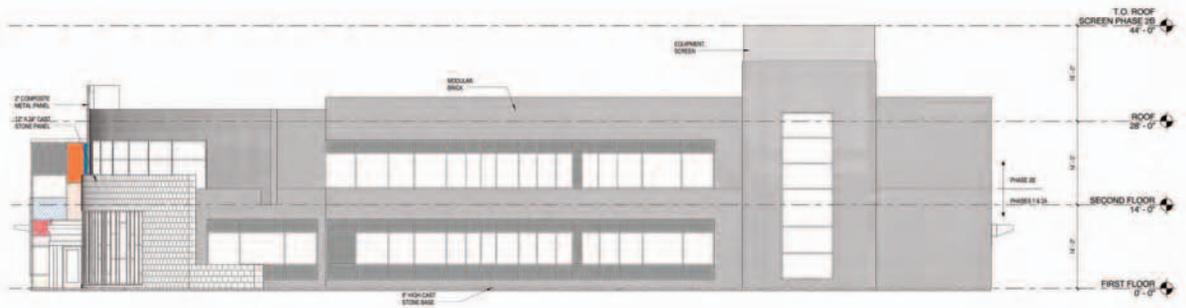
Source: Burt, Hill



B1 ELEVATION- NORTH PHASE 2B
1/8" = 1'-0"



D1 ELEVATION- NORTH PHASE 2B (CONTT.)
1/8" = 1'-0"



F1 ELEVATION- EAST PHASE 2B
1/8" = 1'-0"

Figure 26: North and East Elevations of the Pediatric Medical Office

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: Burt, Hill

D. Access, Parking and Drop-off Locations

The SNF is permitted three curb cuts off Plainsboro Road for driveways. A vehicular drop-off area shall be provided at the building entrance, with access via the two westernmost driveways. The easternmost driveway of the site shall link to parking areas to the south and east of the SNF complex, and shall also provide emergency access to the ARC to the east.

Parking lots for the SNF may be located at the northeastern portion of the parcel, bordering Plainsboro Road and the ARC site, and along the southerly side of the site behind the buildings and parallel to the Millstone River. Parking in this latter area shall be heavily screened from view of the Public Park to the south and west by means of a double row of trees bordering the parking lot, as well as groundcover landscaping.

Loading areas shall be screened from public view.

E. Bicycle Facilities

The SNF shall provide showers and changing rooms to make it easier for staff to commute by bicycle. Bicycle racks shall be provided at an entrance to the building under a protected canopy, or within a secure locked room within the building.

F. Landscaping

A 25-foot wide vegetated buffer containing a mix of deciduous and evergreen trees and shrubs shall be provided around the parking area located to the south of the building, wherever it is adjacent to the PP. Within this buffer area, plantings to a height of at least eight (8) feet at planting shall be provided. Within the parking lot, landscaped islands shall be provided, spaced at regular intervals to allow for the planting of trees and other vegetation. At least one (1) tree shall be planted for every 15 spaces within the parking lot. The landscaped islands within the parking lot shall also have low ground cover plantings and grasses and/or lawn.

5.1.3 Adult Retirement Community (ARC)

A. Orientation

The multiple buildings that make up the ARC shall be clustered around a central parking area. The primary pedestrian and vehicular entrances to each building shall all face this central area. For each building, the facade that faces the central area shall be considered the “primary” facade.

B. Massing

Each of the ARC buildings shall be broken into a series of bays. On each facade of each building, no more than two bays may be up to 100 feet in width. Additional bays on each facade shall be less

than 50 feet in width. A bay is defined primarily by a substantial break in plane having a projection or recession proportionate to the height and mass of the building/structure. Additional features shall also be used to distinguish adjacent bays, such as different cladding, fenestration, texture, color and height.

To help visually ground each ARC building, the base shall be highlighted. The height of the base is flexible and depends on the building design; the base could extend from two to twelve or more feet in height, and could encompass only part of the first level or the entire first one or two levels. The base shall be differentiated from the upper portion of the facade by a change in materials, thicker cladding, or a horizontal element such as a canopy, cornice or belt trim.

Enclosed aerial hallways or open air breezeways linking all residential buildings within the ARC are encouraged for the benefit of residents; however, care shall be taken in creating an attractive and inviting at-grade pedestrian experience within the grounds of the site.

C. Materials and Transparency

The materials and architectural style used for ARC buildings shall be consistent and strive to relate the buildings to one another, creating a unified development. However, buildings shall not be identical or repetitive, so as to provide variation and interest. The materials and style shall also relate to the existing development within the Redevelopment Area.

ARC buildings shall also have at least 25% of each façade as glazing, including on the first level façades. However, if the first level is used for parking opaque glazing may be used. Glazing above the first level shall be 100% transparent or tinted, but not opaque or mirrored.

D. Open Space

All land that is not surface parking or buildings shall be landscaped with lawn, native groundcover plantings, and trees. Any surface drainage basins shall have a naturalistic, irregular form and be landscaped to serve as an attractive visual amenity at all times.

A network of walking trails shall be provided that extends through the grounds of the entire ARC. Walking paths shall connect the front door of all buildings to Plainsboro Road, to all parking areas, and to the Public Park to the west. Furthermore, a looped walking path shall be provided around the entire perimeter of the ARC, as described earlier in **Section 4.1.4(i)**.

E. Access, Parking and Drop-off Locations

The primary vehicular access to the ARC shall be from Plainsboro Road, roughly in the middle of the site's street frontage. A secondary vehicular access route—primarily for emergency access—shall connect to the driveway of the SNF to the west, and thence to Plainsboro Road. Such an emer-

gency access connection may utilize grass concrete blocks to limit impervious coverage and to maintain the open lawn area between the two complexes.

Surface parking for the ARC is limited to the area at the center of the development. Additional parking for residents shall be provided in structured parking within each building.

The central parking lot shall be divided into two main parking fields, each no more than 125 feet wide. Some open landscaped areas shall be provided to provide relief from the blacktop. An architectural feature such as a gazebo or fountain shall be provided at the terminus of the main driveway to provide a focal point from Plainsboro Road.

Structured parking for facility residents and/or staff shall be located at grade, fully below grade, or partially below grade, underneath the buildings. No more than one level of at- or below-grade parking is permitted within each building. Separate parking garage structures and building extensions containing only parking are prohibited.

- ✧ The façade of that portion of the building containing structured parking at grade or partially below grade shall have window openings of a size and spacing that mimics those of the residential floors above, base definition and cladding that creates visual interest and variety within the parking-level facade, and glazing and/or decorative grilles to block headlamp glare.

Guest parking shall be provided within a central surface parking lot surrounded by buildings with screening in between. Separate drop-off areas are not required.

Loading areas shall preferably be located within parking at grade below the buildings, and directly accessible to service elevators. No loading areas shall be located between a building and an exterior property line, and if located outside, shall be screened from view.

F. Parking Lot Landscaping

Within the central surface parking lot, a landscaped area of roughly 20 feet in width shall be created and planted with a row of deciduous trees, spaced 30 feet on center or closer. The landscaped area shall also have low ground cover plantings and grasses and/or lawn. Pedestrian walkways shall extend down the both sides of the landscaped area, providing a safe walking area for seniors.

At the outer edge of the two main parking fields, further landscaped areas shall be provided, similarly planted and treed. Landscaped areas should be designed without curb, and below the grade of parking lots, so as to serve as precipitation infiltration areas.

Additional requirements for surface parking lot landscaping are provided in Section 5.2.3.

G. Bicycle Facilities

The ARC shall provide showers and changing rooms to allow employees or staff of the ARC to commute by bicycle. Bicycle racks shall be provided at the entrance to one or more buildings, under a protected canopy, or within secure locked room(s) within building(s).

5.1.4 Pediatric Medical Office (PMO)

Figures 25 through 27, which show conceptual elevations of the PMO buildings from all sides when fully completed, as well as renderings of Phases 1 and 3 of the PMO building from the southwestern corner of the PMO, is provided for reference purposes in connection with the design guidelines and standards in this district.

Unless explicitly noted, the standards below apply to individual phases of the PMO building as well as to the final, completed building. That is, assuming that the building is built in two phases, the first phase must meet all the requirements in this section on its own, unless otherwise noted, without depending on later phases.

A. Orientation

The PMO building shall have its primary pedestrian entrance facing south toward Plainsboro Road. Secondary pedestrian entries shall also be provided.

Most of the surface parking spaces shall be placed to the north or south of the building. Up to thirty (30) percent of the parking supply may also be provided east of the building; however, no parking is permitted west of the building, between the building and the Green Corridor.

B. Massing

Viewed in plan, the PMO building's south facade shall have a compound "S" curve form that echoes the HMOC across Punia Boulevard, and the primary pedestrian entry to the building shall be located at the inflection point of this compound curve. This entry shall be in a projecting foyer with two sets of doors, aligned to face west and east. The projecting foyer shall have minimum dimensions of 20 feet wide (parallel to the south facade) and 15 feet deep.



PHASE 1



PHASE 3

Figure 27: Rendering of the Pediatric Medical Office from Southwest

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Phillips Preiss Grygiel LLC 2013

Source: Burt, Hill

The west facade of the first phase, and the east facade of the final phase, shall each be broken into smaller elements through changes in facade massing such as projections, recesses, or bays. No continuous length of building along these facades shall exceed 125 feet without an intervening change in plane of at least ten (10) feet wide by ten (10) feet deep.

No massing changes are required in the north facade.

C. Materials & Transparency

The facades of the PMO building shall be clad in brick and/or masonry units. Each facade shall use just one of these as its primary material, with the other used only for accent trim.

The base shall be highlighted with a contrasting stone or concrete band that projects outwards relative to the main facade plane, and has a height of six (6) to thirty-six (36) inches.

The projecting foyer in the south facade that houses the primary pedestrian entry shall have deep rigid canopies over both doors, constructed of metal and/or glass. Such canopies shall project at least five (5) feet from the facade.

Other pedestrian entries to the building shall also be shielded with rigid canopies, but such canopies are only required to be at least three (3) feet deep.

Mosaic Glazing

Mosaic glazing refers to the use of varied-size glazing panels that create a visual composition of colors alternating with clear or neutral-tinted glass. This approach is required of the south and west facades, as follows.

- ⌘ In the south facade, mosaic glazing shall occupy at least sixty (60) percent of the overall length of the first floor facade, and such glazed areas shall have a minimum glazed height of ten (10) feet. Within the projecting entry mass, however, glazing shall occupy at least 80 percent of the south facade area, and such glazed areas shall have a minimum glazed height of 14 feet.
- ⌘ In the west facade, mosaic glazing shall occupy at least forty (40) percent of the overall length of the first floor facade, and such glazed areas shall have a minimum glazed height of ten (10) feet.
- ⌘ Mosaic glazed areas in the south and west facades shall be enlivened through the alternating use of colored glazed panels in primary and/or other bright, contrasting colors, alternating with clear or grey-tinted glazed panels. Between 25 and 50 percent of the required glazed areas in the south facade and between 10 and 20 percent on the west facade shall be of colored, tinted glazing that allows light to pass through.
- ⌘ Opaque or spandrel glass may not be used within the required mosaic glazed areas.

Standard Glazing

Standard glazing refers to the requirement for transparent, translucent, clear or tinted glazing in neutral shades.

- ⤴ For the south facade's first floor, no standard glazing is required.
- ⤴ For the west facade's first floor, clerestory windows or full-height windows⁹ shall be provided in areas that lack mosaic glazing so that in total, at least 90 percent of the entire western facade's length includes glazing.
- ⤴ For the north facade's first floor, clerestory windows or full-height windows shall be provided along at least eighty (80) percent of the facade length.
- ⤴ For the east facade's first floor in the final phase building, clerestory windows or full-height windows shall be provided along at least fifty (50) percent of the facade length.
- ⤴ On the upper floor of the second phase building, for all facades, standard glazing shall be provided for at least 25 percent of the area of each facade,

Roof Screening

The PMO building shall have a parapet along all sides that extends at least two (2) feet above the roof. Rooftop mechanical equipment shall be fully screened behind metal panels of sufficient height to match or exceed that of the equipment being screened.

D. Access, Parking and Drop-off Locations

Primary vehicular access to the PMO building shall be from Punia Drive via two driveways. An additional driveway shall connect the PMO parking lot to those of the ORC component, and thence to Hospital Drive. The entrance driveway closest to the PMO building shall have a curving alignment that relates to the curved south facade of the PMO building.

A one-way vehicular drop-off loop shall be provided adjacent to the south facade.

Parking to the south of the building shall be oriented with bays perpendicular to the south facade. Bays shall radiate outwards from the south pedestrian entry, with flared islands between each bay. At the center of the south parking area, a walkway shall be provided that links the building's south entry to walking paths around the retention basin, and thence to sidewalks along Punia Boulevard and Plainsboro Road.

Parking to the north of the building shall be oriented with bays perpendicular to the north facade. Parking to the east of the building shall be oriented with bays parallel to the north east.

⁹ “**Clerestory windows**” are defined as horizontally-proportioned windows that are placed high within a floor, to allow light into a space while preserving privacy. Clerestory windows shall have a minimum height of two (2) feet and shall be transparent, translucent, clear or tinted.

“**Full height windows**” are defined as those that occupy at least 75 percent of the height of a given floor.

Pedestrian crosswalks shall be provided where circulation aisles and driveways intersect a sidewalk or pathway that links to a public sidewalk or a building entry.

Loading areas shall be located on the north side of the building and shall be screened from public view.

E. Open Space

The two largest open spaces for the PMO are the Green Corridor along its western edge and the open space and landscaped berms along its southern edge.

Refer to Section 4.2.2 for regulations applicable to the Green Corridor.

Open Space and Landscaped Berms

In order to relate to the retention basin at the HMOC to the west and the Green Corridor along Punia Boulevard, an open space area and landscaped berms shall be provided south of the PMO. A secondary purpose is to screen the parking lot in front of the PMO buildings and to create separation and scenic views of the Redevelopment Area from Plainsboro Road. The berms shall be densely planted with trees, shrubs and low plantings, providing the appearance of vegetation in a natural state. The berms shall serve as a buffer and screen for the parking lot in front of the PMO buildings; however, such plantings are intended to screen the view of parked cars only, and views of the building itself shall be provided from Plainsboro Road. The planting theme of the Green Corridor shall also be continued within an open space along the Plainsboro Road frontage to the south of the berms at a width of not less than 50 feet.

Courtyard

A smaller courtyard-type open space, intended for use only by PMO patients, visitors, and staff, shall be provided at the southwest corner of the PMO building, and shall be framed on two sides by the building. A building entrance shall open directly into this open space. The space shall be landscaped with shade trees and low groundcover and may include a variety of seating areas as well as structures or equipment utilized for physical therapy. A low, curved wall shall be provided along the open space's outer perimeter so that it is clearly a space for building, not public, use.

Temporary Landscaping and Pathways

The portion of the PMO site slated for the second phase of the building shall be attractively landscaped until construction commences on such phase. Landscaping shall include a turf or lawn, as well as meandering stone dust pathways. A few shade trees, while not mandatory, are encouraged, as are landscaped berms that create an undulating terrain up two to three feet in height. Outdoor seating areas such as benches are also encouraged to be provided.

Parking Lot Landscaping

Two types of landscaping are required in the south parking area:

- ⤴ Along the southerly side of the parking area, a row of shade trees shall be planted, with an average spacing of no more than 40 feet apart on center. A shade tree shall also be provided at both ends of each parking row.
- ⤴ Within the parking areas, the islands between each bay of parking shall be landscaped with lawn or low groundcover. In addition, the island that contains the walkway to the retention basin area shall include a double row of shade trees, in a species that provide an accent or contrast to the other tree species in this area.

Two types of landscaping are required in the north and east parking areas:

- ⤴ Along the northern perimeter, a row of shade trees shall be spaced no more than 30 feet apart on center. Minor variations in spacing are permitted, but in no case shall such plantings be aggregated in only one or a few areas.
- ⤴ Rows of shade trees, with each tree spaced no more than thirty (30) feet apart on center, shall be planted between bays of parking, within center dividing islands. A shade tree shall also be provided at both ends of each parking row.

F. Bicycle Facilities

The PMO building shall provide showers and changing rooms to allow staff to commute by bicycle. Bicycle racks shall be provided either: at the employees' entrance under a protected canopy, within a secured locked room within the building; or with bike lockers.

5.1.5. Office/Residential Complex (ORC)

A. Orientation

All buildings within the ORC shall be oriented to face either towards Punia Boulevard or Hospital Drive or both. No parking or circulation driveways are permitted between the buildings and Punia Boulevard and Hospital Drive; instead, this area will comprise the Green Corridor (see Section 4.2.2). A pedestrian entry for each building shall open directly onto the Green Corridor. Another pedestrian entry for each building shall open onto the main parking area for the building.

B. Massing

The longer two facades of each building shall be divided into at least three (3) bays, with each bay not to exceed 200 feet in length. A bay is distinguished primarily by a substantial break in plane having a projection or recession proportionate to the height and mass of the building or structure.

Pedestrian entries to the ORC buildings shall be located in projecting masses that give visual cues to enable visitors to easily find the entries. Contrasting materials and higher proportions of glazing shall be used in the entry masses.

C. Materials & Transparency

The materials and architectural styles used for the ORC buildings shall strive to relate the buildings to each other, creating a unified development. The materials and styles shall also relate to existing development within the Redevelopment Area.

The ORC buildings shall each have glazing occupying at least twenty-five (25) percent of each facade, as measured at each individual level of the buildings. Of this required minimum:

- ✧ At the ground level, at least 50 percent of the required minimum glazing shall be clear, un-tinted, transparent glass; this type of clear glazing is particularly encouraged around entryways and overlooking landscaped open space areas in order to strengthen the buildings' relationship to its surroundings.
- ✧ At upper levels, all of the required minimum glazing shall be clear, transparent, translucent, or tinted, but may not be opaque or mirrored.

If additional glazing beyond the 25 percent minimum required is provided at any level of a facade, it may be of any style: clear, tinted, translucent, opaque or mirrored.

D. Access, Parking and Drop-off Locations

No more than two driveways shall be provided to each of the buildings in the ORC. Driveway connections to the driveway which links the PMO and ORC on the eastern side of the Redevelopment Area shall also be used as alternative access. Drop-off locations shall not be adjacent to the Green Corridor, but on the interior side of all buildings. Loading areas shall be located in the same manner and screened from public view.

E. Parking Lot Landscaping

Parking lots for the ORC shall be broken up with landscaped islands so that connected parking areas do not exceed 120 feet by 135 feet in area (equivalent to four double-loaded rows of parking spaces containing a maximum of 15 spaces per row, for a total of 60 spaces). The landscaped islands shall have low groundcover plantings and/or grasses, as well as rows of deciduous trees planted no more than 30 feet on center. The design of such parking lots should consider the possible installation of a solar energy canopy system over a portion of the parking.

F. Open Space

The primary open space of the ORC site is the Green Corridor at the northwestern perimeter. Design standards for the Green Corridor were provided in Section 4.2.2.

The ORC site shall include a surface water feature designed as a reflecting pool, which may be located partially or fully within the Green Corridor, at the southern end of the ORC. The reflecting pool shall be designed as an attractive amenity that holds water at all times, and if possible should also detain stormwater. The reflecting pool shall be at least 4,000 square feet in size and at least 25 feet wide. Walkways within the ORC site shall connect directly to the reflecting pool, and seating areas and perimeter landscaping shall be provided to further increase its appeal as a place to relax.

No stormwater detention or retention areas are permitted within the Green Corridor of the ORC, but may be permitted at the eastern end of the site, beyond the parking areas.

G. Bicycle Facilities

All buildings within the ORC shall provide showers and changing rooms to allow workers to commute by bicycle. Bicycle racks shall be provided at the entrance to each building under a protected canopy, or within a secure locked room within each building.

5.1.6 Day Care Complex

Figures 28 through 30 provide conceptual elevations of the Day Care facility for reference purposes with respect to the design guidelines and standards in this district.

A. Orientation

Two buildings are permitted in the Day Care area. Buildings shall be at least 25 feet apart, and the space between the buildings shall be designed as an attractive open space with landscaping.

Building parking spaces shall be provided at the rear of the buildings, closest to the corner of Scudders Mill Road and Campus Road, and shall be accessed via driveways from Hospital Drive, with up to two curb cuts on Hospital Drive. Driveways shall be located on the exterior sides of the buildings, that is, not at the central open space referenced above.

Primary building entrances shall be located at either the rear or the interior facades of the buildings. The “rear facade” shall be the facade closest to the corner of Scudders Mill Road and Campus Road, where parking spaces are located. The “interior facade” shall be the façade facing the central open space between the two buildings.



ELEVATION AT ENTRY

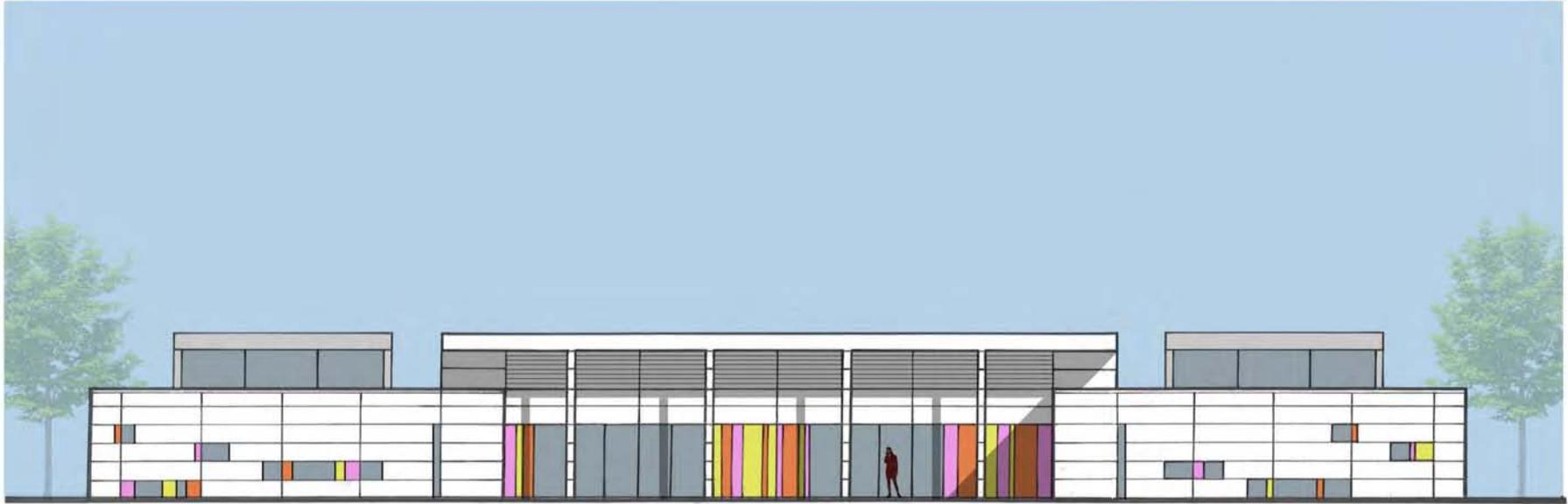
DISCOVERY YEARS

J. ROBERT HILLIER
LAND USE STRATEGY EXECUTION

Figure 28: Conceptual Elevation of Daycare Complex Entry

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: J. Robert Hillier



ELEVATION FROM PLAYGROUND

DISCOVERY YEARS

J. ROBERT HILLIER
LAND USE STRATEGY EXECUTION

Figure 29: Conceptual Elevation of Daycare Complex from Playground

Amended Redevelopment Plan for the Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: J. Robert Hillier



LONG ELEVATION

DISCOVERY YEARS

J. ROBERT HILLIER
LAND USE STRATEGY EXECUTION

**Figure 30: Conceptual Long
Elevation of Daycare Complex**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: J. Robert Hillier

B. Massing

The maximum permitted facade length in the direction roughly parallel to Hospital Drive is 125 feet. Buildings that are wider than 125 feet parallel to Hospital Drive, at either the front or rear elevation shall be broken up into smaller components along each elevation by means of bays so that any one building mass element is less than 125 feet wide. A bay, in this case, is distinguished primarily by a substantial break in plane having a projection or recession proportionate to the height and mass of the building or structure.

C. Materials & Transparency

Buildings shall have glazing occupying at least twenty-five (25) percent of each facade, as measured at each individual level of the buildings. Of this required minimum:

- ▲ At the ground level, at least 50 percent of the required minimum glazing shall be clear, un-tinted, transparent glass; this type of clear glazing is particularly encouraged around entryways and overlooking landscaped open space areas in order to strengthen the buildings' relationship to its surroundings.
- ▲ At upper levels, all of the required minimum glazing shall be clear, transparent, translucent, or tinted, but may not be opaque or mirrored.

If additional glazing beyond the 25 percent minimum required is provided at any level of a facade, it may be of any style: clear, tinted, translucent, opaque or mirrored.

D. Access, Parking and Drop-Off Location

No more than two curb cuts shall be permitted for the DCC, both of which shall be on Hospital Drive. No drop-off or pickup lane is required if the applicant can provide sufficient spaces to accommodate a park and walk-in operation. However, to the extent a drop-off/pickup lane is provided, such a lane shall be no more than 12 feet wide, and shall provide for sufficient stacking vehicles behind the drop-off/pickup location. An additional lane of the same dimensions, and separated by a curbed island at least 3 feet wide, shall be provided for access to a parking lot. Both prior to the queuing portion of the pickup and drop-off lane, and after the drop-off/pick-up location, the two lanes may be combined, provided they are one-way, and in which case the combined lanes shall be no more than 18 feet in width.

The drop-off/pickup location shall be on the north and/or east side of the building. The parking lot shall be located north and east of the drop-off/pick up location and lane, and shall be separated from the lane by a landscaped island of at least 10 feet in width.

To the extent that any loading is provided, such space shall be located on the rear side of the building, not within the yard fronting Hospital Drive.

E. Open Space and Landscaping

The central open space between the two buildings shall include landscaping such as shade trees and ground cover landscaping, and may also include decorative paving, and seating areas.

Any courtyards, patios, or other open areas created by building massing should also be landscaped in a similar fashion, and provide seating areas. Playground equipment for the day care facility is encouraged. Such a play area shall be enclosed by a fence, which shall complement in color and material the architecture and building finishes of the Day Care building. The play area may be located adjacent to Hospital Drive, but shall be set back at least 20 feet, with the intervening area planted with solid vegetative screening at least four (4) feet high.

A solid screen of vegetative plantings at least four feet high shall be provided adjacent to the property line on Scudders Mill Road and Campus Road to screen the driveways and parking areas located within the DCC.

At-grade, pedestrian walkways shall connect the primary building entries to each other, to the existing pedestrian path along the northern boundary of the property, and to Hospital Drive. Any required bio-retention areas shall be provided at the rear of buildings, not along Hospital Drive.

Underground detention basins are also permitted, either below parking or landscaped open space.

F. Bicycle Facilities

Each building within the DCC shall provide showers and changing rooms to allow workers to commute by bicycle. Bicycle racks shall be provided at the entrance to each building under a protected canopy, or within a secure locked room within each building.

5.1.7 Hospital Auxiliary District (HAD)

A. Landscaping

A 50-foot wide vegetated buffer containing a mix of deciduous and evergreen trees and shrubs shall be provided adjacent to the parking area along the perimeter of the parcel adjacent to Scudders Mill Road and the exit ramp of Route 1. Within this buffer area, a solid screen of evergreen plantings at least eight (8) feet high at planting shall be provided. A 30-foot wide vegetated buffer with similar plantings shall be provided along the remainder of the HAD frontage on the Route 1 exit ramp.

Within the parking lot, landscaped islands shall be provided, spaced at regular intervals to allow for the planting of trees and other vegetation. At least one (1) tree shall be planted for every 15 spaces within the parking lot. The landscaped islands within the parking lot shall also have low ground cover plantings and grasses and/or lawn.

B. Walkways

A walkway or sidewalk at least five feet wide shall be provided adjacent to the driveway which connects the parking lot in the HAD to the driveway extension of Hospital Drive. Pedestrian crosswalks directing pedestrians from the parking lot within the HAD to the pedestrian walkways in the HMOC shall be provided.

5.2 PARKING STANDARDS

5.2.1 Parking Ratios

Unless as otherwise provided below, parking shall be provided according to the standards in §85-44 of the Township's *Subdivision and Site Plan Review* regulations.

Minimum parking ratios within the Redevelopment Area shall be as set forth below. These ratios are minimum requirements only; it shall be incumbent on the owner of each site to meet all of the parking needs of the particular use on site. Without limiting this requirement, the Planning Board in its discretion may grant deviations from the ratios contained herein, provided it is demonstrated to the satisfaction of the Planning Board, through appropriate studies prepared by a Licensed New Jersey Professional Engineer, that a lesser parking ratio will be adequate to serve the site.

Hospital Medical Office Complex (HMOC)

Hospital:	4 spaces for every bed
Medical Office Building:	4 spaces for every 1,000 square feet of gross floor area

Skilled Nursing Facility (SNF)

All parking shall be provided in surface parking lots.

Skilled Nursing Facility:	1.0 space per bed
Dialysis Center:	5.0 spaces per 1000 square feet of gross floor area

Adult Retirement Community (ARC)

Surface parking may only be provided at the center of the cluster of buildings. Additional parking spaces shall be provided in structured parking underneath the main living spaces of buildings. For more information, see Section 5.1.3D.

Independent Living:	1.5 spaces for every unit
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Pediatric Medical Office (PMO)

Hospital and Medical Offices:	5.0 spaces per 1,000 square feet of gross floor area
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Office/Residential Complex (ORC)

Congregate Units:	1.0 space per unit
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Assisted Living/Dementia Units:	1.0 space per bed
Hospice:	1.0 space per bed
General Office:	4 spaces per 1,000 square feet of gross floor area
Research:	4 spaces per 1,000 square feet of gross floor area

Day Care Complex (DCC)

Day care:	1 space per 8 students, plus 1 space per teacher/staff
Special education:	5 spaces per 1,000 square feet of gross floor area
Adult day care:	1 space per 12 students, plus 1 space per staff

At the discretion of the Planning Board, portions of the parking provided may be “banked” (set aside) based upon credible evidence that the total number of required spaces may not be needed. The banked parking area shall be fully landscaped with turf and/or ground cover plantings. Following approval, the property owners or the Township may cause the remainder of the spaces to be paved, if required. Banked spaces shall count towards the development’s calculation of impervious cover-age.

5.2.2 Structured Parking Design¹⁰

Where applicable, the roof of any structured parking provided underneath, above, or at the interior of a building shall be covered, paved, and landscaped to serve as a courtyard or terrace for building occupants.

Bicycle racks and/or lockers shall be provided in a dedicated area within the garage near the pedestrian entrance/exit to the garage. Such bicycle storage shall be designed to be expandable in response to demand for additional facilities.

Freestanding parking garages shall be architecturally compatible with the primary buildings in each subarea.

Where a freestanding structured parking garage is either permitted or required (i.e., in the HMOC), the garage shall be fully hidden or architecturally screened from view by one or more of the following features:

- The garage should be attached to and hidden behind office space, for example located at the interior of a building, mechanically ventilated as necessary;
- The garage façade shall be articulated like a non-curtain wall office building, with regularly-spaced openings covered with grilles and/or translucent glazing, mechanically ventilated as necessary. The articulation should disguise the low ceilings and sloping ramps of the garage.
- The garage façade shall be hidden behind a separate screen, mounted at a distance from the façade (so as to allow natural ventilation) that mimics a curtain wall building or a more traditional building

¹⁰ See also Sections 3.2 and 5.1.1F.

with separate windows. The screen should extend above the roof level of the garage, so as to disguise any roof parking and mechanical equipment.

- The garage façade shall be covered with metal architectural mesh or other creative covering that disguises the levels of parking and ramps, preferably creatively lit at night.
- The developer is encouraged to install a solar energy canopy system over the parking on the top of the garage.

The design goal underlying the standards above is to ensure such structures are designed as an integral component of the buildings they serve, and that the architectural and functional elements of such structures, including the use of materials, fenestration, screening, and vehicular and pedestrian circulation/access, are handled in a manner that complements and enhances the overall operation and appearance of the healthcare campus.

5.2.3 Surface Parking Design

Unless a more stringent requirement is provided elsewhere in this document for a particular component of this Plan, the following standards shall apply.

The perimeters of parking lots and islands between parking bays shall be planted with trees spaced regularly or in pairs with four (4) trees per 100 linear feet, or tighter spacing.

Parking lot drive aisles that are oriented perpendicular to building façades shall align with building entrances.

Islands defining the edge of rows of parking and bordering drive aisles shall be planted with one tree for every 40-foot length of island, or portion thereof) with a minimum of one tree at the end of each parking row. Islands may be raised with curbs, or depressed and curbless if required for stormwater management.

For parking lots exceeding 200 feet in width or length, one or more pedestrian pathways shall be provided with one located through the approximate center of the parking lot. The pathways, where appropriate, shall align with entrances to adjacent buildings. The pathways shall be at least six (6) feet wide and landscaped with a pair (2) of trees (one on each side of the pathway) for every forty (40) linear feet or portion thereof, as well as with bushes and flowering plants.

Solar canopies over parking spaces are encouraged in all surface parking lots that have sufficient solar access to make installation economically feasible. (See Figure 31, which shows solar canopies being installed in the surface parking lot of the HMOC.)



Figure 31: Solar Canopies under construction in HMOG Parking Lot

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013
Source: PPG 2012

5.3 STORAGE AND MECHANICAL AREAS

5.3.1 Mechanical and Utilities

A. Exposed Equipment and Structures

Exposed storage areas, service areas, truck loading areas, and accessory buildings and structures shall be set back, screened with plantings, and fenced in an appropriate and attractive manner to prevent their incongruity with buildings and the surrounding property.

All above-ground utility structures, machinery, and the mechanical controls for same, including but not limited to generators, transformers, junction boxes, lift stations, electrical meters, condensers, and signal boxes, shall be set back at least 20 feet from the paved cartway of a street or driveway, or masked by building elements in a manner consistent with the design of the building. A wall with venting for mechanical rooms shall not be permitted along facades facing streets, pathways, and open spaces, unless they are treated architecturally to be similar to or compatible with adjacent structures, and to the extent necessary, screened by landscaping from public view.

Outdoor storage of materials, equipment, or goods of any kind is prohibited.

B. Rooftop Equipment

Rooftop mechanical equipment, including elevator shafts, HVAC ductwork and equipment and satellite dishes and other communication equipment, shall be fully screened from view behind a parapet or other architectural feature that is an integral part of the overall building design.

C. Floodproofing

Any mechanical equipment located within the 100-year flood plain (below the 100 year flood elevation) shall be housed in a flood-proof structure.

5.3.2 Telecommunications Equipment

Telecommunications antennas shall be permitted subject to the provisions contained in Article XXII, Wireless Telecommunications Facilities of the Township of Plainsboro's *Zoning* regulations. With the exception of the antenna, all parts and components of personal communications antennas, satellite dishes, and television and radio antennas shall be screened from view regardless of elevation, or shall be disguised within an enclosed structure. The screening shall be designed as part of the overall design theme of the building to which it is associated.

Antenna panels for personal communications services (PCS) may be attached to the parapet of a building provided they are indistinguishable in color and texture from the building material and do not extend above the top of the parapet to which they are attached.

5.4 GREEN DESIGN

5.4.1 Green Building Design

Buildings should be designed with progressive green design, such as green roofs, use of daylight, and use of shade screens and landscaping to reduce solar gain. Solar canopies in surface parking lots are also encouraged (see Figure 31).

Green roofs should be employed where possible and where roofs are visible from upper-level floors. All buildings should be designed to LEED standards as follows:

- All new buildings in the HMOC, SNF and ORC, should be designed to LEED standards.
- The new ARC development should be designed to LEED NC standards.
- The renovated existing buildings within the HMOC should be designed to standards for LEED Commercial Interiors or LEED Existing Buildings.

In all of the above cases, LEED certification shall not be mandatory, but is encouraged.

5.4.2 Green Lighting Design

All exterior lighting, including building lighting, streetlights, and pathway and parking lot lighting, should comply with “dark sky” standards that reduce energy use and eliminate glare and nighttime light pollution. Recommended light fixtures and additional information are available from the International Dark-Sky Association (IDA), www.darksky.org.

5.5 SIGNAGE

Signage design shall be driven by the need for simplicity and clarity in communication. Exterior signage shall be primarily for wayfinding purposes rather than advertising. Signage shall be designed as a comprehensive and complementary system for each of the components within the Redevelopment Area. While all site signage shall include common design elements, including the use of select colors and materials, individual occupants of the Redevelopment Area are encouraged, where appropriate, to also include design elements that will contribute to making their signage unique and distinctive, yet consistent within the framework of the signage design principles in the Redevelopment Plan. Building mounted signage may be tailored to meet the individual needs of future users; however, with the exception of the hospital, such signage shall be designed to be visible primarily to on-site visitors. Signage shall be only as large as functionally necessary, and monument signs are preferred over freestanding pylon signs. Refer to §101-141, Letters O and G, of the Township of Plainsboro's *Zoning* regulations, for additional information. Developers shall prepare a comprehensive, coordinated signage package to include building identification signs, directional/wayfinding signs, parking/circulation and other regulatory signs. All such signs shall be of high quality and shall complement the overall design character of the redevelopment area.

See Figures 32 and 33, which show the monument entry signs for the University Medical Center of Princeton at Plainsboro and the Merwick Care Center at the Plainsboro Road entrances.

5.6 STREET DESIGN

Except as provided in the street cross sections in Chapter Four of this Redevelopment Plan, the following sections of the Township's *Subdivision and Site Plan Review* regulations shall govern: §85-20, Streets; §85-21, Street Signs; §85-22, Sidewalks; 85-23, Curbing; §85-26, Monuments; §85-27, Streetlighting; §85-28, Drainage; and §85-28.1, Stormwater Management.

5.7 LANDSCAPE DESIGN

In addition to the standards provided below, development shall comply with the landscape regulations found in §101-141, Letters A and H, in Article XII, PMUD Planned Unit Development Regulations, of the Township of Plainsboro's *Zoning* regulations; as well as in §85-43C of the Township's *Subdivision and Site Plan Review* regulations.

5.7.1 Building Landscaping

The perimeters of buildings within the Redevelopment Area shall be softened by ground cover plantings of flowers, shrubs, trees and grasses. Additional specific requirements follow.

A. Hospital

A respite area shall be created adjacent to the emergency department entry planned within the north façade of the "Diagnostics and Treatment Platform building. A landscaped area that serves the entry to the education center shall be created in front of (to the south of) Building Two.

A healing garden shall also be planted in front of (to the south of) Building Two and shall be accessible from the cancer center. Building Two's form shall enclose the healing and educational gardens to its south. Additional gardens shall be provided at the northern border of Building Two.

Courtyards within the HMOC shall be landscaped.

5.7.2 Trees

Refer to the following sections of the Township's *Subdivision and Site Plan Review* regulations for additional regulations: §85-24, street shade trees; §85-30, on-site tree preservation; §85-30.1, reforestation; and §85-30.2, tree preservation during construction.



**Figure 32: Monument Entry Sign to
UMCPP at Plainsboro on Plainsboro Road**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013

Source: PPG 2012



**Figure 33: Proposed Permanent
Monument Entry Sign for
Merwick Care Center on Plainsboro Road**

Amended Redevelopment Plan for the
Princeton HealthCare System at Plainsboro Site

Phillips Preiss Grygiel LLC 2013

Source:

5.8 UTILITIES AND INFRASTRUCTURE

5.8.1 Utilities

All public utilities shall be provided in accordance with §85-29 of the Township of Plainsboro *Subdivision and Site Plan Review* regulations.

A water supply system shall be provided in accordance with §85-32 of the Township of Plainsboro *Subdivision and Site Plan Review* regulations.

A sewer system shall be provided in accordance with §85-33, Sanitary Sewers and Septic Systems, of the Township of Plainsboro *Subdivision and Site Plan Review* regulations.

5.8.2 Stormwater

Unless as otherwise provided herein, stormwater drainage shall be provided in accordance with §85-28 and §28-28.1 of the Township of Plainsboro's *Subdivision and Site Plan Review* regulations.

The site shall be designed for the retention and infiltration of stormwater in order to minimize runoff into the area's storm sewer system. Within the public park, plantings and vegetation shall be designed to slow and filter overland flow. Locations for surface detention and retention basins shall be limited to the easterly side of the ORC adjacent to the State owned lands, and the southerly portion of the ARC to limit views from the public rights of way. The basin adjacent to the ORC shall be heavily buffered to eliminate views from all directions. Stormwater management facilities may also be provided below parking areas. Where possible, overflow parking areas, emergency vehicle routes, and other paved areas of low usage or activity shall be paved with porous pavers or "grass-crete" to allow for infiltration.

Refer to §101-141, Letters E and J, in Article XII, PMUD Planned Unit Development Regulations, of the Township of Plainsboro's *Zoning* regulations; as well as §85-25, Topsoil Protection, §85-28, Drainage; and §85-28.1, Stormwater Management, of the Township's *Subdivision and Site Plan Review* regulations for additional regulations.

5.8.3 Solid Waste and Recycling

Each building shall be designed to provide adequate storage of solid waste disposal, including provisions for recycled materials, within buildings or parking facilities. Each multifamily or non-residential building shall have at least one trash and recycling pick-up location.

All exterior trash and recycling locations shall be enclosed and located in a manner which is obscured from view from parking lots, streets, parks and adjacent buildings by a fence, wall, planting, or combination thereof.

Refer to §101-141, Letter L, in Article XII, PMUD Planned Unit Development Regulations, of the Township of Plainsboro's *Zoning* regulations for additional requirements.

6.0 REDEVELOPMENT PLAN PHASING

Due to the large size of the Redevelopment Area and the magnitude of the facilities to be developed for the Hospital Medical Office Complex (HMOC), the redevelopment is anticipated to occur in phases. With Phase I complete (see Table 2), the redevelopment is likely to continue as indicated below:

- Phase 1, which is complete, included development of the first phase of the HMOC, full development of the SNF, and completion of the PP.
- Phase 2 will likely include development of the PMO, the DCC, the ARC, the ORC and HAD.
- Phase 3 will likely include expansions of the hospital bed tower and medical arts pavilion in the HMOC.

The projected amount of development that has already occurred and that is anticipated to occur in each of the succeeding phases, is shown in **Table 2**.

Note that each component within Phase 2 may be initiated but not completed prior to the completion of the hospital and medical offices in the HMOC. The phasing plan is therefore meant to provide a framework for the development of the entire Redevelopment Area, but factors such as the real estate market and outside agency approvals are likely to dictate the sequence and pace of the buildout.

Insofar as the phasing of improvements is concerned, they would fall into 2 categories. In the first category, certain required public improvements—the required streets, bikeways and walkways, and the provision of space within the Public Park—are tied to components of the amended Redevelopment Plan, as illustrated in Chapter 4 of the Amended Open Space and Circulation Plan. Prior to the granting of certificates of occupancy for these components, the required roads/parking areas, walkways/bikeways, and improvements along the public streets and within each district shall be completed and found acceptable by the Township.

The second category of improvements—which are items such as public utility improvements (water, sewer, gas, electricity), improvements to existing intersections or roadways, and the number and type of affordable housing units either built on-site or otherwise provided, shall be determined at site plan approval for each component of the amended Redevelopment Plan and as per the Redevelopment Agreement between the Township Committee of the Township of Plainsboro and the redeveloper.

Table 2: Redevelopment Phasing: Components with Existing and Anticipated Floor Plans

Phase	Component	Floor Area		Status
		Component	Total	
1	A. <u>Hospital Medical Office Complex</u>			
	• Hospital (Phase I)	636,000 sf		Complete
	• Fitness Center	48,000 sf		U/C*
	• Medical Office Building (Phase I)	146,870 sf		Complete
	• Central Utility Plant	10,854 sf		Complete
	• Cogeneration Facility	4,928 sf		Complete
	• Solar Canopy Array System	0 sf		Complete
	Subtotal	846,652 sf		
	B. <u>Skilled Nursing Facility</u>	107,030 sf		Complete
	C. <u>Public Park</u>	0 sf		Complete
	TOTAL		953,682 sf	
2	A. <u>Pediatric Medical Office</u>			
	Phase 1	25,000 sf		--
	Phase 2	25,000 sf		--
	Phase 3	<u>50,000 sf</u>		--
	Subtotal	100,000 sf		--
	B. <u>Day Care Complex</u>			
	Phase 1 (Day Care)	13,000 sf		--
	Phase 2 (Special Education)	<u>13,000 sf</u>		--
	Subtotal	26,000 sf		--
	C. <u>Adult Retirement Community</u>	898,000 sf		--
D. <u>Office and Residential Complex</u>	180,000 sf		--	
E. <u>Hospital Auxiliary District</u>	1,000 sf		--	
TOTAL		1,205,000 sf		
3	A. <u>Hospital Medical Office Complex</u>			--
	Hospital (Phase 2)	322,000 sf		--
	Medical Office Building (Phase 2)	<u>93,000 sf</u>		--
	Structured parking in accordance with 3.2 and 5.1.1F		415,000 sf	
TOTAL				
	TOTAL (ALL PHASES)		2,573,682 sf	

* U/C – Under construction

7.0 REDEVELOPMENT ACTIONS

7.1 PROPERTIES TO BE ACQUIRED

All of the properties in the Redevelopment Area have been acquired by the designated redeveloper, Princeton HealthCare System, a New Jersey Nonprofit Corporation. Therefore, no additional property acquisitions are necessary.

7.2 OTHER ACTIONS

Other actions may need to be undertaken to further the goals of the amended Redevelopment Plan. These may include, but shall not be limited to:

- Clearance of dilapidated, deteriorated, obsolete or underutilized structures or uses where necessary
- Construction of new structures or other improvements
- Provisions for public infrastructure necessary to service and support new development, including new roads
- Environmental remediation
- Vacation of easements as may be necessary for redevelopment.

7.3 RELOCATION

Implementation of the amended Redevelopment Plan does not require the displacement or relocation of any residents within the Redevelopment Area.

7.4 LINKAGES

The Redevelopment Plan recognizes that the Redevelopment Area offers an opportunity to create substantial employment along the Route 1 corridor and to provide physical, economic, social, cultural and educational linkages to the community of Plainsboro. The amended Redevelopment Plan encourages the following types of linkages:

- Pedestrian and bikeway links to Plainsboro Village Center, the new library, and the wider Plainsboro sidewalk and bikeway system.
- Community educational and outreach programs which provide awareness about health and wellness, including partnering with such organizations as Plainsboro Library, the West Windsor-Plainsboro School District, religious organizations and corporations and research organizations in the region.
- Coordinating and implementing improvements in transportation/transit alternatives, including private shuttles and Bus Rapid Transit.

- Economic linkages to medical and other non-medically related office and retail space within Plainsboro outside of the Redevelopment Area, and to businesses in Plainsboro, through direct purchase of goods and services by the hospital and through increased expenditures within Plainsboro by employees and residents from within the Redevelopment Area.

8.0 PLAN CONSISTENCY REVIEW

As required by the Local Redevelopment and Housing Law, this section describes the relationship of this amended Redevelopment Plan to (a) the land uses and zoning contained within the adjacent municipalities which may be affected by the amended Redevelopment Plan—West Windsor Township and Princeton, both of which are contiguous municipalities in Mercer County; (b) Middlesex County planning documents; and (c) the New Jersey State Development and Redevelopment Plan.

8.1 RELATIONSHIP TO ADJACENT MUNICIPALITIES

8.1.1 West Windsor

The Plainsboro Township-West Windsor municipal boundary is situated along the Millstone River which, with its trees and floodplains, provides a substantial buffer between the two communities. The adjoining land uses are fully compatible with those proposed for the Redevelopment Area in Plainsboro. The properties in West Windsor across the Millstone River from the Redevelopment Area are zoned Research & Development (R&D), which encourages office development similar to the medical office uses envisioned for the Redevelopment Area.

The proposed development within the Redevelopment Area will be generally consistent with the West Windsor lands on the southern bank of the Millstone River. In addition, the West Windsor Master Plan has designated a major open space/greenbelt buffer along the Millstone River, which is consistent with the planned Public Park component of the amended Redevelopment Plan.

8.1.2 Princeton

The boundary between Plainsboro Township and Princeton runs along Carnegie Lake and the Millstone River, both of which provide a substantial, natural buffer between adjoining land uses, with the former bordering Plainsboro on the western side of Route 1. The Redevelopment Area, as such, is not directly contiguous to the Princeton municipal boundary. This buffer is further enhanced by the park system along the Millstone River. Zoning in this portion of Princeton includes an E-1 Educational and Multi-Family Residential District and an R-5 Residential District which allows single-family homes on lots of a minimum of a half-acre in size. The Princeton Community Master Plan encourages moderate-density residential development in this area.

The permitted development within the Redevelopment Area will be generally consistent with Princeton on the southern and western bank of the Millstone River and Carnegie Lake, respectively.

8.2 RELATIONSHIP TO MIDDLESEX COUNTY PLANS

8.2.1 Middlesex County Growth Management Strategy Phase II (1992) and Phase III (1995) Reports

In 1992 and 1995, the Middlesex County Planning Board adopted its Phase II and Phase III Growth Management Reports, respectively. Both reports present a comprehensive analysis of the infrastructure capacity and planning issues facing anticipated growth areas of Middlesex County. Growth areas within the County, relative to Plainsboro, include the Route 1 corridor, the Redevelopment Area's western boundary. A case study of the Plainsboro-Forrestal Center area was presented in the Phase II report, with the purpose of devising progressive growth management and planning approaches to:

- Equitably balance the long-term costs of infrastructure between the government and the private sector;
- Reduce and/or prevent traffic congestion;
- Protect natural resources, preserve open space, and provide recreational opportunities;
- Assure that new patterns of development do not compromise the existing quality of life in our communities; and
- Address emerging water quality and air quality objectives being established by the State and Federal government.

The overall objective of this amended Redevelopment Plan satisfies all of the key growth management and planning approaches recommended by the Phase II Report. This amended Redevelopment Plan calls for a progressive and comprehensive planning approach to continuing redevelopment within the Route 1 corridor.

8.2.2 Middlesex County Comprehensive Transportation Study (December 2003)

In December 2003, the Middlesex County Planning Board adopted the Middlesex County Comprehensive Transportation Study, which addresses the present and future transportation needs of the county through a comprehensive analysis of the past, present, and future demographic and employment framework. The Study specifically notes that the Princeton Forrestal Center, a growing employment area, is not well served by transit, and along with the other major employment centers in Plainsboro/South Brunswick, could benefit from a Bus Rapid Transit (BRT) service, which would "increase mobility and alleviate congestion in Central New Jersey, including southern Middlesex County."

This amended Redevelopment Plan is consistent with the Study, as progressive transportation alternatives, such as traffic congestion mitigation measures and the planned BRT stop, are planned within the Redevelopment Area. The Study also cites the overall need for improved intra-municipal transit circulation. The proposed BRT stop will certainly improve transit availability and access within Plainsboro.

8.2.3 Middlesex County Open Space and Recreation Plan (October 10, 1995)

In October 1995, the Middlesex County Planning Board adopted the Middlesex County Open Space and Recreation Plan, which contains open space and recreation goals and objectives, and recommendations for future land acquisition for incorporation into the County open space system. This amended Redevelopment Plan is consistent with the goal and objective for land conservation as well as the provision of open space for recreational purposes. Specifically, this amended Redevelopment Plan satisfies these objectives through the provision of both public and private open space with the Redevelopment Area, most notably the publicly accessible Public Park component.

8.2.4 Middlesex County Context Report: Strategic Planning Areas/Endorsed Plans

Middlesex County is completing strategic area plans that cover the County's growth regions. The strategic plan process is being utilized by the County in order to assure that continued growth will not overwhelm the infrastructure systems of the County and reduce the quality of life for County residents. The location of the Redevelopment Area along the Route 1 corridor and the availability of the necessary infrastructure capacity to support the site's redevelopment is consistent with Middlesex County's strategic planning objectives.

8.3 RELATIONSHIP TO STATE DEVELOPMENT AND REDEVELOPMENT PLAN

On March 1, 2001, the State Planning Commission adopted the New Jersey State Development and Redevelopment Plan (NJSDRP). This new plan supersedes the document that had been in place since June 12, 1992. The NJSDRP, which municipalities are encouraged but not mandated to follow, is a framework for guiding both regional and municipal growth in the State in the future. Since the NJSDRP is currently undergoing an amendment the 2001 Plan is still in effect.

This amended Redevelopment Plan is consistent with the NJSDRP, in that the Redevelopment Area is located within the Suburban Planning Area (PA-2) which the State Plan describes as follows:

“Suburban Planning Areas are or will be served by regional infrastructure, except that, outside of Centers and major transportation corridors, there is limited, if any, availability of alternative modes of transportation to the automobile. These areas have generally been designated for growth in municipal master plans. As development expands, these services will become increasingly available if planned properly”

This amended Redevelopment Plan is consistent with the policies for PA-2, which includes objectives to:

1. Provide for much of the state's future development;
2. Promote growth in Centers and other compact forms;
3. Protect the character of existing stable communities;

4. Protect natural resources;
5. Redesign areas of sprawl;
6. Reverse the current trend toward further sprawl.

Moreover, this amended Redevelopment Plan furthers the following specific NJSDRP policy objectives:

- (1) **Land Use:** Enhance economic and agricultural viability and rural character by guiding development and redevelopment into Centers. In the Environs, maintain and enhance agricultural uses, and preserve agricultural and other lands to form large contiguous areas and greenbelts around Centers. Development and redevelopment should use creative land use and design techniques to ensure that it does not conflict with agricultural operations, does not exceed the capacity of natural and built systems and protects areas where public investments in farmland preservation have been made. Develop and redevelopment in the Environs should maintain or enhance the character of the area.
- (2) **Housing:** Provide for a full range of housing choices primarily in Centers at appropriate densities to accommodate projected growth, recognizing the special locational needs of employees and minimizing conflicts with agricultural operations. Focus multifamily and higher density single-family housing in Centers.
- (8) **Redevelopment:** Encourage appropriate redevelopment in existing centers and existing developed areas that have the potential to become Centers, or in ways that support Center-based development to accommodate growth that would otherwise occur in the Environs. Redevelop with intensities sufficient to support transit, a broad range of uses, efficient use of infrastructure, and design that enhance public safety, encourage pedestrian activity, reduce dependency on the automobile and maintain the rural character of Centers.
- (10) **Public Facilities and Services:** Phase and program for construction as part of a dedicated capital improvement budget or as part of a public/private development agreement the extension or establishment of public facilities and services, particularly wastewater systems, to establish adequate levels of capital facilities. Encourage private investments and facilitate public/private partnerships to provide adequate facilities and services, particularly wastewater systems, in Centers.

9.0 GENERAL PROVISIONS

9.1 EASEMENTS

In addition to any other required approvals, no building shall be constructed over a public easement in the Redevelopment Area without prior written approval of the Engineer of the Township of Plainsboro.

9.2 SITE PLAN AND SUBDIVISION REVIEW

Prior to commencement of construction or rehabilitation within the Redevelopment Area, site plans for the construction and/or rehabilitation of improvements within the Redevelopment Area, prepared in accordance with the requirements of the Municipal Land Use Law (N.J.S.A. 40:55D-1 et seq.), shall be submitted by the applicants for review and approval by the Planning Board of the Township of Plainsboro so that compliance with the amended Redevelopment Plan can be determined.

All applications for site plan shall be accompanied by a vehicular trip generation analysis demonstrating to the satisfaction of the Township Engineer that the vehicular trips that are projected to be produced by the proposed development are equal to or less than those that were projected as part of the Traffic Impact Study prepared by TRC Engineers Inc. dated December 21, 2012. Where the projected trip generations exceed those anticipated in the aforementioned study, a revised Traffic Impact Study conforming to the parameters to be agreed to and approved by the Township Engineer's Office shall be submitted.

Any subdivision of lots and parcels of land within the Redevelopment Area shall be in accordance with the requirements of this amended Redevelopment Plan and the Subdivision and Site Plan Review regulations of the Township of Plainsboro, and the supplemental submission requirements listed below.

9.2.1 Supplementary Submission Requirements

In accordance with the procedure established within the originally adopted Redevelopment Plan, in addition to submitting plans and information as specified in the Subdivision and Site Plan Review regulations of the Township of Plainsboro, the following supplemental plans and information shall accompany each site plan and/or subdivision application.

- a. An amended **general land use plan** at a scale of a minimum of 1 inch equals 100 feet indicating the tract area and general locations of both the existing redeveloped portions of the Redevelopment Area along with additional proposed land uses to be included in the Redevelopment Area. The total number of dwelling units and amount of nonresidential floor area to be provided, including the proposed land area to be devoted to residential and nonresidential use shall be set forth, as well as the proposed number of parking spaces to be provided. In addi-

tion, the proposed types of additional nonresidential uses to be included in the Redevelopment Area shall be set forth, and the land area to be occupied by each proposed use shall be estimated. The density and intensity of use of the entire Redevelopment Area—both completed and/or approved as well as proposed—shall be set forth, and a residential density and a non-residential floor area ratio shall be provided.

- b. An amended **circulation plan and traffic impact analysis**. The amended circulation plan shall show the general location and types of transportation facilities, including facilities for pedestrian and bicycle access that have been completed as well as those that are proposed, within the Redevelopment Area, as well as proposed improvements to the existing transportation system outside the Redevelopment Area—to the extent they have yet to be completed—including a phasing plan for the implementation of such improvements. The traffic impacts of the Redevelopment on the surrounding road network shall be set forth in the traffic impact analysis.
- c. An amended **open space plan** showing the proposed land area and general location of open space and any other land area that has already been set aside and any additional land area that is proposed to be set aside for conservation and recreational purposes and a general description of improvements proposed to be made thereon. The amended open space plan shall also include any amendments to the approved Open Space Plan which include any changes or additions to the plan for the operation and maintenance of all open space and recreational lands within the Redevelopment Area. Such a plan shall include any additional lands proposed to be deeded to the Township as public open space.
- d. An amended **utility plan** indicating the need for and showing the proposed location of any changes or additions to sewage and water lines, any changes or additions to drainage facilities necessitated by the physical characteristics of the site, any changes or additions to proposed methods for handling solid waste disposal, and any changes or additions to the plan for the operation and maintenance of proposed utilities.
- e. An amended **stormwater management plan** setting forth the proposed method of controlling and managing stormwater on the site, and any changes or additions to such plan.
- f. An amended **environmental inventory**, to the extent necessary, including a general description of any changes or additions to the vegetation, soils, topography, surface water, wetlands, floodplains and stream corridors of the site, existing manmade structures or features and the probable impact of the redevelopment on the environmental attributes of the site.
- g. An amended **community facility plan** to the extent necessary, indicating the scope and type of supporting community facilities, including but not limited to, recreational, educational or cultural facilities included within the Redevelopment Area, and the extent to which such facilities may be open to Plainsboro Township residents or the public in general.

- h. An amended **housing plan** outlining any changes or additions to the originally approved housing plan, setting forth the number of housing units to be provided and the extent to which any housing obligation assigned to the municipality pursuant to P.L. 1985, c. 222 (C.52:27D-301 et al.) will be fulfilled by the development.
- i. An amended **local service plan** indicating any changes or additions to the originally approved local service plan, setting forth those public services which the redeveloper proposes to provide and which may include, but shall not be limited to, water, sewer, cable, Internet, electricity, gas, telephone, and solid waste disposal.
- j. An amended **fiscal impact report** describing the anticipated demand on municipal services to be generated by any changes and/or additions to the amended Redevelopment Plan and any other financial impacts to be faced by the municipality or school districts as a result of the completion of the redevelopment. The fiscal report shall also include a detailed projection of property tax revenues which will accrue to the county, municipality and school district according to the proposed sequence of development, and following the completion of the redevelopment in its entirety. Such report shall show the fiscal impact at each phase or stage of the development, to insure that at no time will there be a negative fiscal impact on the community.
- k. An amended proposed **phasing plan** and timing schedule for construction, to the extent that it is necessary.

No construction or alteration to existing or proposed construction shall take place until a site plan reflecting such additional or revised construction has been submitted to and approved by the Planning Board. This pertains to revisions or additions prior to, during and after completion of the improvements.

9.3 AMENDED REDEVELOPMENT PLAN AND EXISTING ZONING

This amended Redevelopment Plan shall supersede any zoning that previously applied to the properties located within the designated Redevelopment Area.

9.4 ADVERSE INFLUENCES

No use or reuse shall be permitted which, when conducted under proper and adequate conditions and safeguards, will produce corrosive, toxic or noxious fumes, glare, electromagnetic disturbance, radiation, smoke, cinders, odors, dust or waste, undue noise or vibration, or other objectionable features so as to be detrimental to the public health, safety or general welfare.

9.5 NON-DISCRIMINATION PROVISIONS

No covenant, lease, conveyance or other instrument shall be affected or executed by the Mayor and Township Committee of the Township of Plainsboro or by a redeveloper or any of his successors or

assignees, whereby land within the Redevelopment Area is restricted by the Township Committee of the Township of Plainsboro, or the redeveloper, upon the basis of race, creed, color, sexual orientation, or national origin in the sale, lease, use or occupancy thereof. Appropriate covenants, running with the land forever, will prohibit such restrictions and shall be included in the disposition instruments. There shall be no restrictions of occupancy or use of any part of the Redevelopment Area on the basis of race, creed, color or national origin.

9.6 DURATION OF THE PLAN

The provisions of this Plan specifying the redevelopment of the Redevelopment Area and the requirements and restrictions with respect thereto shall be in effect for a period of thirty-five (35) years from the date of approval of this amended Redevelopment Plan by the Township Committee of the Township of Plainsboro or such other time as set forth in any redevelopment agreement (as hereafter required), between the Township of Plainsboro and any redeveloper of any portion of the Redevelopment Area.

9.7 DEVIATION REQUESTS

The Planning Board may grant deviations from the regulations contained within this amended Redevelopment Plan where, by reason of exceptional narrowness, shallowness or shape of a specific piece of property, or by reason of exceptional topographic conditions, preexisting structures or physical features uniquely affecting a specific piece of property, the strict application of any area, yard, bulk or design objective or regulation adopted pursuant to this amended Redevelopment Plan, would result in peculiar practical difficulties to, or exceptional and undue hardship upon, the redeveloper of such property. The Planning Board may also grant such relief in an application relating to a specific piece of property where the purposes of this amended Redevelopment Plan would be advanced by a deviation from the strict requirements of this Plan and the benefits of the deviation would outweigh any detriments. No relief may be granted under the terms of this section unless such deviation or relief can be granted without substantial detriment to the public good and without substantial impairment of the intent and purpose of the amended Redevelopment Plan. Any deviation from use, height or a “d” variance under Municipal Land Use Law N.J.S.A. 40-55D-1 et seq. shall require an amendment to this amended Redevelopment Plan by the Township Committee. An application for a deviation from the requirements of this amended Redevelopment Plan shall provide public notice of such application in accordance with the requirements of public notice as set forth in NJSA 40:55D-12.a. and b.

9.8 AFFORDABLE HOUSING

As of the date of this amended Plan, the New Jersey Supreme Court has yet to rule on the growth share methodology that formed the basis for the ‘third round’ regulations adopted in 2008 by the Council on Affordable Housing (COAH). Accordingly, those regulations remain invalidated, and have not been replaced by any new regulations. Moreover, no legislative or other solution has been enacted. It is therefore not known, nor can it be anticipated, when new regulations will be adopted

nor how the Township’s affordable housing obligation will be determined and calculated going forward. It is the intent of this amended Plan that in no event shall construction within this Redevelopment Area pursuant to this amended Plan result in an additional obligation to the Township to provide affordable housing pursuant to policy or regulations of COAH or any other such administrative agency of the State of New Jersey to whom such authority has been designated by the New Jersey legislature, unless such additional obligation is met by the redeveloper of such project pursuant to a redevelopment agreement between the redeveloper and the Township Committee. To account for the uncertainty of how the Township’s future obligation will be determined, while at the same time ensuring to the greatest extent possible that any affordable housing obligation attributable to, or triggered by, development within the Redevelopment Area be addressed by the developer, affordable housing shall be provided within the Redevelopment Area as follows:

ARC	At least 10 percent of all units shall be affordable to very low, low and moderate-income individuals as defined by COAH or its successor agency/entity.
ORC	<p>At least 10 percent of any assisted living or congregate care units shall be affordable to very low, low and moderate-income individuals as defined by COAH or its successor agency/entity.</p> <p>All other development in the ORC shall be subject to the 2.5% nonresidential development fee set forth in the “Statewide Non-residential Development Fee Act,” <i>N.J.S.A. 40:55D-8.1 to -8.8</i> (“Act”), unless expressly exempt therefrom pursuant to the terms and provisions of the Act or other applicable State law or regulation.</p>
DCC, SNF, PMO and HMOC	All development in the DCC, SNF, PMO and HMOC shall be subject to the 2.5% nonresidential development fee set forth in the “Statewide Non-residential Development Fee Act,” <i>N.J.S.A. 40:55D-8.1 to -8.8</i> (“Act”), unless expressly exempt therefrom pursuant to the terms and provisions of the Act or other applicable State law or regulation.

9.9 UTILITIES

9.9.1 Availability of Utilities

The following utility agencies have indicated that they have the capacity and are willing to provide service to the proposed redevelopment within the amended Redevelopment Area.

Natural Gas	—	<i>Public Service Electric and Gas (PSE&G) Company</i>
Electric Power	—	<i>Public Service Electric and Gas (PSE&G) Company</i>
Potable Water	—	<i>New Jersey American Water Company</i>
Telephone Service	—	<i>Verizon</i>

9.9.2 Sanitary Sewer System Assessment

The South Brunswick Utility Department controls the sanitary sewer conveyance system in the Redevelopment Area. The projected sanitary sewer flow rate for the proposed redevelopment in accordance with the originally approved Redevelopment Plan within the Redevelopment Area was preliminarily estimated to be approximately 260,000 gallons per day. The sanitary sewer flow generated within the Redevelopment Area is conveyed by gravity from the Redevelopment Area to the existing meter chamber which is located on the southeastern portion of the Redevelopment Area just north of the Millstone River. An existing 42-inch sewer interceptor line conveys the sanitary sewer flow across the Millstone River. There remains adequate conveyance capacity in the 42-inch sewer interceptor to convey both the existing as well as the projected flow rate from the full redevelopment of the Redevelopment Area.

The Stony Brook Regional Sewerage Authority controls the sanitary sewer conveyance system from Plainsboro Township to the River Road Treatment Plant. The Stony Brook Regional Sewerage Authority has determined that the existing sanitary sewer conveyance systems, including pump stations and interceptor lines, have adequate capacity to convey both the existing as well as the projected sanitary sewer flow from future redevelopment to the River Road Treatment Plant. In addition, the River Road Treatment Plant still has capacity to accept and treat the additional amount of projected sanitary sewer flow resulting from this amended Redevelopment Plan.

10.0 OTHER PROVISIONS

In accordance with N.J.S.A. 40A:12A-1 et seq., known as the Local Redevelopment and Housing Law, the following statements are made:

- The originally adopted Redevelopment Plan delineated a definite relationship to local objectives as to appropriate land uses, density of population, and improved traffic and public transportation, public utilities, recreation and community facilities and other public improvements. The Plan also laid out various programs and strategies that needed to be implemented in order to carry out the objectives of this Plan. Thus far, development within the Redevelopment Area has complied with the adopted Plan. Future redevelopment shall likewise comply with such objectives and continue to implement such programs and strategies.
- This amended Redevelopment Plan lays out the proposed land uses and building requirements for the portions of the Redevelopment Area which have not been redeveloped or completed.
- While the amended Redevelopment Plan provides specific guidance for the redevelopment of those portions of the Redevelopment Area which have not been redeveloped or completed, any applicant seeking any development approval shall be required to submit applications for such approval to the Planning Board. During the review of any such applications, the typical Planning Board and Development Review Committee (DRC) processes shall be followed, and nothing precludes the DRC and the Planning Board from requiring refinements and modifications of the proposed development.
- If any section, paragraph, division, subdivision, clause or provision of this amended Redevelopment Plan shall be adjudged by the courts to be invalid, such adjudication shall only apply to the section, paragraph, division, subdivision, clause or provision so judged, and the remainder of this amended Redevelopment Plan shall be deemed valid and effective.

11.0 REDEVELOPMENT AGREEMENT(S) REQUIRED

No redevelopment of the Redevelopment Area shall occur, whether consistent with this amended Redevelopment Plan or not, except in accordance with a redevelopment agreement between such redeveloper and the Township of Plainsboro, duly approved by the Plainsboro Township Committee.

12.0 PROCEDURE FOR AMENDING THE APPROVED PLAN

This amended Redevelopment Plan may be further amended from time to time upon compliance with the requirements of law. Any party requesting a future amendment is required to submit such request to the Township Committee. A fee of \$500 shall be paid by the party requesting such further amendment, unless the request is issued from an agency of the Township. The Township Committee, at its sole discretion, may require the party requesting such further amendments to prepare a study of the impact of such amendments, which study must be prepared by a Professional Planner, licensed in the State of New Jersey. In addition, the Township Committee, at its sole discretion, may require that the party requesting such further amendments establish an escrow account with the municipality adequate to allow the Township to retain a Professional Planner and any other necessary professionals, licensed in the State of New Jersey, to identify, review and/or prepare proposed further amendments that might be needed, together with any and all necessary documentation related thereto to ensure that the intent of this amended Redevelopment Plan will not be compromised.